

CITY COUNCIL AGENDA: MARCH 5, 2013

PUBLIC HEARING

SUBJECT: PACIFIC RIM COMMERCIAL MIXED USE PROJECT (PRC 2012-002), GENERALLY LOCATED WEST OF THE SOUTHWEST CORNER OF HENDERSON AVENUE AND NEWCOMB STREET

SOURCE: COMMUNITY DEVELOPMENT DEPARTMENT - PLANNING DIVISION

COMMENT: The applicant is requesting approval of General Plan Amendment 2012-002, Zone Change 2012-002, and Tentative Parcel Map 2012-002 to facilitate development of a vacant 23.4± acre site with a commercial mixed use project. The applicant is also requesting consideration of Conditional Use Permit 2012-002 contingent upon approval of the General Plan Amendment and Zone Change. With the requested entitlements, the applicant proposes to develop a commercial mixed use project including 168 multi-family residential (apartment) units, 39,400± square feet of commercial uses, an 18,000± square foot two-story office building, and a 518± unit personal storage facility. The proposed project is generally located west of the southwest corner of Henderson Avenue and Newcomb Street.

The General Plan Amendment and Zone Change applications are required to accommodate a project of this scale and function at the subject site. The 23.4± acre site has multiple General Plan land use designations and zoning classifications including:

Site Area	General Plan Designation	Zoning Classification	Proposed
7.38± acres	High Density Residential	RM-3, High Density Residential	Commercial Mixed Use
14.77± acres	Medium Density Residential	RM-2, Medium Density Residential	Commercial Mixed Use
1.23± acres	Neighborhood Commercial	CN, Neighborhood Commercial	Commercial Mixed Use

A tentative parcel map is needed to configure four (4) parcels for site development as proposed. If the General Plan and Zone Change are approved, the applicant is requesting approval of a Conditional Use Permit to approve the 518± unit mini-storage facility. Conditions of approval have been added to the project approvals to ensure that the entire project will be developed as one project regardless of what sequence the project phasing follows. The market is envisioned to drive the sequencing of the phases. Specifically, the Zone Change has been conditioned to facilitate this objective.

The applicant has requested a waiver of development fees; however, as the City has not approved such a waiver in the past, staff finds no precedent to support approval of a fee waiver for the proposed project. Staff informed the applicant of

DD ~~201~~ APPROPRIATED/FUNDED MB CM J

ITEM NO 116

the 10-year development fee payment plan for all fees except school fees pursuant to Resolution 37-2012. Traditionally, the fee payment plan has only been used for commercial or low-income housing developments. As the proposed project includes a mix of commercial, office, and market-rate housing development, staff supports the use of the 10-year development fee payment plan as a logical extension of the program. It has been added as a condition of approval to the Parcel Map. If the Council chooses not to authorize this, it can be removed from the resolution.

When recent street improvements were completed along Henderson Avenue, there was a concrete reimbursement agreement approved for the property requiring payback upon development. The applicant has requested to waive this reimbursement requirement, which staff does not support. However, where those improvements are required to be removed, and new improvements are required to accommodate the development, that portion of the cost is deducted from the payback requirement; staff believes this provides an equitable balance.

**ENVIRONMENTAL DETERMINATION:** An environmental initial study was prepared for the project. On February 5, 2013, the Environmental Coordinator made the determination that a mitigated negative declaration would be applicable and appropriate for the project under CEQA guidelines. A mitigation monitoring plan has been developed and shall be incorporated into the zone change as development obligations of the project. To date, one letter of comment has been submitted. Dr. John Snaveley, Superintendent of Porterville Unified School District submitted a letter expressing concerns over the pedestrian and vehicular congestion at the intersection of Henderson Avenue and Newcomb Street. A copy of the letter is attached to the complete staff report for consideration.

**RECOMMENDATION:** That the City Council:

1. Adopt the draft resolution approving the Mitigated Negative Declaration;
2. Adopt the draft resolution approving General Plan Amendment 2012-002;
3. Approve the draft ordinance adopting Zone Change 2012-002 contingent upon approval of General Plan Amendment;
4. Waive further reading of the draft ordinance, approving the Zone Change and order it to print;
5. Adopt the draft resolution containing findings and conditions in support of approval of the Tentative Parcel Map 2012-002; and
6. Adopt the draft resolution containing findings and conditions in support of approval of Conditional Use Permit 2012-002 for the personal storage facility.

**ATTACHMENTS:** Complete Staff Report

PUBLIC HEARING - STAFF REPORT

TITLE: PACIFIC RIM COMMERCIAL MIXED USE PROJECT (2012-002),  
 GENERALLY LOCATED WEST OF THE SOUTHWEST CORNER OF  
 HENDERSON AVENUE AND NEWCOMB STREET

OWNER/APPLICANT: Henderson MU-V  
 Paul Owhadi  
 29610 Heathercliff Road, Suite 411  
 Malibu, CA 90265

AGENTS:	The Vincent Company Scott Vincent 1500 West Shaw, Suite 30 Fresno, CA 93711	Winton & Associates Jim Winton 150 West Morton Avenue Porterville, CA 93257
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PROJECT LOCATION: Generally west of the southwest corner of Henderson Avenue and Newcomb Street

SPECIFIC REQUEST: The applicant is requesting approval of General Plan Amendment 2012-002, Zone Change 2012-002, and Tentative Parcel Map 2012-002 to facilitate development of a vacant 23.4± acre site with a commercial mixed use project. The applicant is also requesting consideration of Conditional Use Permit 2012-002 contingent upon approval of the General Plan Amendment and Zone Change. With the requested entitlements, the applicant proposes to develop a commercial mixed use project including 168 multi-family residential (apartment) units, 39,400± square feet of commercial uses, an 18,000± square foot two-story office building, and a 518± unit personal storage facility. The proposed project is generally located west of the southwest corner of Henderson Avenue and Newcomb Street.

The General Plan Amendment and Zone Change applications are required to accommodate a project of this scale and function at the subject site. The 23.4± acre site has multiple General Plan land use designations and zoning classifications including:

Site Area	General Plan Designation	Zoning Classification	Proposed
7.38± acres	High Density Residential	RM-3, High Density Residential	Commercial Mixed Use
14.77± acres	Medium Density Residential	RM-2, Medium Density Residential	Commercial Mixed Use
1.23± acres	Neighborhood Commercial	CN, Neighborhood Commercial	Commercial Mixed Use

The colors and finish of the proposed development will be consistent throughout the site. The development will have a Spanish mission theme as proposed by the developer. Building finishes

propose terra cotta and pale yellow colored stucco with soft clay, beige and red tones accenting rooftops. Matching signage and awnings compliment building architecture. Open spaces and recreation areas within the residential and commercial areas are provided for enjoyment by residents and patrons. Additionally, incorporated into the design of the project are modern architectural features at entrances, roundabout elements and drive-thru restaurants. Site features include a freestanding clock tower and two water fountain features within the commercial center. Increased landscaping will be provided along the project frontages on Henderson Avenue and throughout the parking lot and median areas. The consistency in design, colors, and architectural features of the project will complement the area and provide transition in development from residential to office and commercial development uses buffered by large landscape areas. The Zone Change has been conditioned to require site design and architectural consistency throughout the development.

**HISTORY:** The proposed project site has been vacant and undeveloped for over 30 years. Prior to the development of surrounding residential uses and clearing of the site, the area was primarily walnut orchards. Over the last several decades the surrounding residential neighborhoods have become established and continued to develop west of the project area towards the City's western limits.

**PROJECT DETAILS:** As stated in the report, there are four components to the proposed mixed use project. The project includes a multi-family apartment complex, a commercial area (including retail shops, fast-food, and a financial pad), an office building, and a personal storage facility.

The multi-family apartment complex is currently permitted under the medium density residential General Plan Land Use Designation and Zoning Classification. The apartments, as proposed, are also permitted under the proposed Commercial Mixed Use General Plan Land Use Designation and Zoning Classification. Frontage and access for the apartments is proposed along Henderson Avenue with secondary access from Newcomb Street across an adjacent site. The 168 units are proposed at a density of approximately 14.3 units per acre, which is consistent with the current density provisions. The complex includes 23 apartment buildings and one office building. The residential component provides a number of recreational amenities for residents, including community buildings (3,300± sq. ft.), three (3) open space areas with a combined average over 17,000 square feet each. Two of the open spaces include a community pool with arbor. In addition, a covered barbeque area is centrally located near the largest open space area that provides residents the ability to cook and eat outdoors. Each unit provides a 50 square foot personal outdoor patio or balcony space. The proposed landscape plans show an abundance of live plants, shrubs, and trees that will be inviting to residents and visitors and encourage the utilization of outdoor space.

The commercial component of the development proposes 39,400± square feet of retail, fast-food and financial/bank services. The commercial area provides two (2) retail buildings (16,800± sq. ft. and 12,000± sq. ft.), two (2) fast-food pads (3,200± sq. ft. each), and one (1) financial pad (4,200± sq. ft.). The existing Neighborhood Commercial node permits all the types of commercial uses proposed, but would not accommodate the proposed size of commercial development on the site. Drive-thru functions in the fast food restaurants would also not be

permitted with the current zoning, but would be accommodated in the proposed Commercial Mixed Use (CMX) zone.

The third component of the project is a proposed two-story 18,000± square foot office building. The office is centrally located and accessed within the entire project. Proposed colors, design, roof and finish are consistent with the rest of the commercial center and residential uses. Parking and landscaping is provided around the entire building.

The final component of the project is the 518± unit personal storage facility. The facility is proposed on the western portion of the overall project tucked primarily behind the commercial center but having some frontage along Henderson Avenue for access. The area consists of fourteen (14) steel storage buildings (matching in color to the development) and one 1,290± square foot office building (consistent in colors, design, roof and features to the entire project). A matching six (6) foot tall concrete block wall with pilasters and stucco finish surrounding the mini-storage is proposed. A block wall is required for separation from residential zoning and uses to the south and west. The block wall around the entire facility will provide additional security and safety for both the surrounding commercial and residential uses.

The proposed CMX General Plan Land Use Designation and Zoning Classification over the entire site will permit all features of the development as proposed. All of the buildings throughout the project provide consistent design, colors, and features. The abundant landscaping and architectural features through the facility present a sense of place while providing a convenient location for multi-family residential uses to commercial, offices, public parks and personal storage facilities. The proposed commercial mixed use project would provide the convenient shopping and services for the surrounding established neighborhoods.

The entire site is pedestrian oriented for access and connectivity between all uses. Although there is a physical separation of uses - a landscaped and plastered six (6) foot tall block wall, the construction of the entire project site will be consistent which will provide a soft transition from use to use.

**STAFF ANALYSIS:** A General Plan Amendment and Zone Change is proposed for the entire site which will change the existing residential and commercial designations to the proposed Commercial Mixed Use designation over the entire site. Due to the nature of the requests the requested applications are generally contingent upon one another in the following order: the environmental is required to be approved prior to any applications, the General Plan Amendment is required to be approved prior to the Zone Change, which is required to be approved prior to the Tentative Parcel Map, which is required prior to the approval of the Conditional Use Permit. All are required to be approved prior to the Master Sign Program. The approvals would allow for uses of the development as proposed. Contingent upon the General Plan Amendment and Zone Change, the proposed personal storage facility requires a conditional use permit within a commercial mixed use zone. All other uses are permitted and allowed by right if the project is approved for development as proposed.

The applicant has requested a waiver of development fees; however, as the City has not approved such a waiver in the past, staff finds no precedent to support approval of a fee waiver for the

proposed project. Staff informed the applicant of the 10-year development fee payment plan for all fees except school fees pursuant to Resolution 37-2012. Traditionally, the fee payment plan has only been used for commercial or low-income housing developments. As the proposed project includes a mix of commercial, office, and market-rate housing development, staff supports the use of the 10-year development fee payment plan as a logical extension of the program. It has been added as a condition of approval to the Parcel Map. If the Council chooses not to authorize this, it can be removed from the resolution.

When recent street improvements were completed along Henderson Avenue, there was a concrete reimbursement agreement approved for the property requiring payback upon development. The applicant has requested to waive this reimbursement requirement, which staff does not support. However, where those improvements are required to be removed, and new improvements are required to accommodate the development, that portion of the cost is deducted from the payback requirement; staff believes this provides an equitable balance.

#### SURROUNDING LAND USES AND ZONING:

North: City- RS-2 (Low-Density Residential) – Single family residential Subdivision  
South: City- RS-2 (Low-Density Residential) – Single family residential Subdivision  
East: City- PK/PS (Parks and Recreation/Public and Semi-public) – Public Park and City Yard  
West: City- RS-2 (Low-Density Residential) - Single family residential Subdivision

#### GENERAL PLAN DESIGNATION AND ZONING:

RM-2 (Medium-Density Residential)  
RM-3 (High-Density Residential)  
CN (Neighborhood Commercial)

ENVIRONMENTAL DETERMINATION: An environmental initial study was prepared for the project. On February 5, 2013, the Environmental Coordinator made the determination that a mitigated negative declaration would be applicable and appropriate for the project under CEQA guidelines. The Initial Study has been transmitted to interested agencies, groups and individuals for review and comment. The review period ran for twenty (20) days from February 8, 2013, to February 28, 2013. A mitigation monitoring plan has been developed and shall be incorporated into the zone change as development obligations of the project. To date, one letter of comment has been submitted. Dr. John Snavely, Superintendent of Porterville Unified School District submitted a letter expressing concerns over the pedestrian and vehicular congestion at the intersection of Henderson Avenue and Newcomb Street. A copy of the letter is attached for consideration.

DATE FILED FOR PROJECT REVIEW COMMITTEE PROCESSING: March 8, 2012

DATE ACCEPTED AS COMPLETE: January 28, 2013

LEGAL NOTICES:

Mitigated Negative Declaration

Date Environmental Document Distributed	Date Notice Published in Porterville Recorder	Date Notice Mailed to Property Owners within 300 feet of Project
February 7, 2013	February 8, 2013	February 7, 2013

RECOMMENDATIONS: That the City Council:

1. Adopt the draft resolution approving the Mitigated Negative Declaration and mitigation monitoring plan;
2. Adopt the draft resolution approving General Plan Amendment 2012-002;
3. Approve the draft ordinance adopting Zone Change 2012-002 contingent upon approval of General Plan Amendment;
4. Waive further reading of the draft ordinance, approving the Zone Change and order it to print;
5. Adopt the draft resolution containing findings and conditions in support of approval of Tentative Parcel Map for 2012-002 contingent upon approval of Zone Change 2012-002; and
6. Adopt the draft resolution containing findings and conditions in support of approval of Conditional Use Permit 2012-002 for the personal storage facility contingent upon approval of General Plan Amendment 2012-002 and Zone Change 2012-002.

ATTACHMENTS:

1. Comment letter from Porterville Unified School District
2. Initial Study and Mitigated Negative Declaration
3. Draft Resolution to adopt the Mitigated Negative Declaration
4. Draft Resolution to adopt General Plan Amendment
5. Draft Ordinance to approve Zone Change (Project Plans are attached to the Ordinance for the Zone Change)
6. Draft Resolution to adopt Tentative Parcel Map
7. Draft Resolution containing findings in support of approval for Conditional Use Permit for the proposed personal storage facility

# PORTERVILLE UNIFIED SCHOOL DISTRICT

*Creating Opportunities: Changing Lives*

## DISTRICT BOARD OF TRUSTEES

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Vice President

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Clerk

600 West Grand Avenue

Porterville, CA 93257  
(559) 793-2400

JOHN SNAVELY, Ed.D.  
DISTRICT SUPERINTENDENT  
(559) 793-2455  
(559) 793-1088 FAX

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Asst. Superintendent  
Instructional Services  
(559) 793-2452  
(559) 793-1083 FAX

February 26, 2013

City of Porterville Council Members  
291 N. Main St.  
Porterville, CA 93257

**RE: Pacific Rim Commercial Mixed Use Project**

Dear Council Members:

My schedule prevents me from attending the City Council meeting scheduled for March 5, 2013, regarding the Pacific Rim Commercial Mixed Use Project located at the southwest quadrant of Henderson Avenue and Newcomb Street. As such, please accept this letter as a request for the City Council to further review the project's impact beyond what staff is currently recommending.

The Porterville Unified School District administration has been engaged in previous conversations and review of the project referenced above with City of Porterville Planning Department staff. In these conversations, we have gone on record expressing concerns with the current amount of vehicular and pedestrian traffic at the intersection of Henderson and Newcomb. It is our expectation that this project will further impact an already congested intersection.

We applaud the staff of the City of Porterville for their effort to acknowledge and respond to our concerns. However, I continue to fear for the safety of the students who are likely to be affected by the increased flow of vehicular and pedestrian traffic from this project.

In particular, it is reasonable to expect that a number of elementary age students will be generated by 168 multiple family dwelling units. It is my understanding that these elementary age children will be attending Oak Grove Elementary located at 1873 West Mulberry Avenue. While the Porterville Unified School District is not responsible for elementary children attending Oak Grove, on behalf of all children, I feel compelled to bring to your attention potential hazards. Having worked in public education for over thirty-years, I believe I have some expertise regarding the walking patterns of students. As I view the project, it is my belief that children and parents are most likely to cross Henderson Avenue at Belmont Street and are not likely to back track to the intersection of Newcomb and Henderson. While we appreciate staff's recommendation to improve the intersection at Henderson and Newcomb, I do not believe this adequately addresses the safety of elementary age students from this development.

On behalf of the Porterville Unified School District, we are not opposed to the project itself; our concerns are over the safety of students. I suspect this is one of the most impacted, if not, the most impacted intersection in our community. It would seem appropriate to encourage any vehicular and pedestrian traffic generated by this development in other directions. We, therefore, request City Council to consider other alternatives to mitigate the increased traffic caused by this project beyond that of improving the intersection of Henderson Avenue and Newcomb Street.

Sincerely,

  
John Snavely, Ed.D.  
Superintendent

JSM  
C: Governing Board of Trustees  
Cores  
COP - Pacific Rim Comm Project

**ATTACHMENT  
ITEM NO. 1**



Pacific Rim  
Commercial Mixed Use Project

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Initial Study & Mitigated  
Negative Declaration

**City of Porterville**  
Community Development Department  
Planning Division

**February 2013**



**ATTACHMENT  
ITEM NO. 2**

**CITY OF PORTERVILLE**  
**ENVIRONMENTAL CHECKLIST**

1. **Project title:** Pacific Rim Commercial Mixed Use Project
2. **Lead agency:** City of Porterville  
291 N. Main Street  
Porterville, CA 93257
3. **Contact person:** Julie Phillips, AICP  
Community Development Manager  
(559) 782-7460
4. **Project location:** Generally the southwest corner of  
Henderson Ave and Newcomb Street  
(Figures 1-4)
5. **Latitude, Longitude:** N 36° 4' 46.1575"  
W 119° 3' 21.1562"
6. **Project sponsor's name  
and address:** Pacific Rim Companies  
29160 Heathercliff Road, Suite 411  
Malibu, CA 90265
7. **General plan designation:** High Density Residential, Medium Density  
Residential, Neighborhood Commercial  
(Figure 3)
8. **Zoning:** High Density Residential (RM-3), Medium  
Density Residential (RM-2) and  
Neighborhood Commercial (Figure 4)
9. **Description of project:** The project proposes to develop  
approximately 23.4 acres of vacant land as a commercial mixed use project consisting of  
four components (Figure 1). The four project components include a multi-family  
apartment complex, a commercial area (including retail shops, fast-food, and banking), a  
two story office building, and a personal storage facility (Figure 2). The proposed project  
would require a General Plan Amendment and Zone Change from high and medium  
density residential and neighborhood commercial uses to commercial mixed use. The  
project site is currently designated and zoned for residential and neighborhood  
commercial uses. Current General Plan land use designations and zoning are represented  
in Figures 3 and 4.

All of the buildings throughout the project provide consistent design, colors, and features resulting in a truly integrated mixed use project. The abundant landscaping and architectural features throughout the proposed development create a sense of place while

providing a convenient location for multi-family residential uses, commercial uses, offices, and personal storage facilities. The proposed commercial mixed use project would serve its own residential population as well as provide convenient shopping and services to the surrounding established neighborhoods.

The entire site is pedestrian oriented for access and connectivity between all uses. Although there is a physical separation of uses (a matching six (6) foot block wall at property lines), the construction of the entire project site would be consistent which would provide a soft transition from use to use. Pedestrian access gates would provide connectivity between uses (excluding the personal storage facility) without requiring foot traffic in drive aisles. Vehicular access for the entire project would be from Henderson Avenue along four driveways.

The multi-family apartment complex is a use permitted under the current medium density residential General Plan Land Use and Zoning Designations. The apartments as proposed are also permitted under the proposed Commercial Mixed Use General Plan land use and Zoning Designation as a part of the larger project. The proposed 176 units would result in a density of approximately 15.17 units per acre; current zoning would allow up to 15 units per acre, and the proposed zoning allows up to 30 units per acre. The residential complex includes 21 eight-unit apartment buildings, two community rooms, and one office building. In addition, the residential development includes three open space areas with a areas of roughly 17,000 square feet each. One of the open spaces includes a community pool with arbor. Additionally, a covered barbeque & picnic area is centrally located near the largest open space area that provides residents the ability to cook and eat outdoors. Each unit provides a 50 square foot personal outdoor patio or balcony space. The proposed landscape plan shows an abundance of live plants, shrubs, and trees that would be inviting residents and visitors and encourage the utilization of outdoor space. Proposed parking for the apartment complex exceeds the Development Ordinance parking standards.

The commercial component of the development proposes 39,400± square feet of retail, fast-food and financial/bank services. The commercial area provides two (2) retail buildings (16,800± sq.ft. and 12,000± sq.ft.), two (2) fast-food pads (3,200± sq.ft. each), and one (1) financial pad (4,200± sq.ft.). The existing neighborhood commercial node permits all the types of commercial uses proposed with exception to the size and drive-thrus. The proposed Commercial Mixed Use General Plan land use designation and Zoning over the entire site would permit the development as proposed. All commercial buildings propose a consistent color palette, design, and architectural theme with the master plan. All commercial buildings would be single story with Spanish themed architecture, featuring mission tiled roofs. Aesthetic improvements include large landscaped frontages along Henderson Avenue and within the parking areas which would provide live plants, shrubs and trees to buffer, shade and screen paved areas. Additionally, two water fountains and a clock tower would provide architectural enhancements to invite patrons and provide a convenient location for retail, banking and food services with drive-thru.

The third component of the project is a proposed two-story 18,000± square foot office building. The office is centrally located and accessible from the commercial component, with designated pedestrian access to the residential component. Proposed colors, design, roof and finish are consistent with the rest of the commercial center and residential uses. Parking and landscaping surround the building.

The fourth component of the project is a 518± unit personal storage facility. The facility is proposed on the western portion of the project, tucked primarily behind the commercial center but having some frontage along Henderson Avenue for access. The area consists of fourteen (14) steel storage buildings (matching in color to the development) and one 1,290± square foot office building (consistent in colors, design, roof and features to the entire project). A matching six (6) foot concrete block wall with plaster and stucco finish surround the mini-storage facility as proposed. A block wall is required for separation from residential zoning and existing uses to the south and west, and would provide additional security and safety for both the surrounding commercial and residential uses. The personal storage facility is provided access along Henderson Avenue; a designated entrance provides patron parking and secured access, as well as a landscaped buffer. The office building further screens the use, offering a continued transition in building design to the adjacent proposed commercial, office and residential uses.

In addition to on-site improvements as described above, the project proposes incorporating raised median islands in two locations on Henderson Avenue to help guide turning movements into and out of the development, as presented in Figure 2.

**10. Surrounding land uses and setting:** The proposed project is located in an urban setting on vacant land that has not been utilized since it was cleared of walnut orchards over 25 years ago. The surrounding uses are primarily residential to the north and south. Churches are located on the adjacent parcels to the east and west. A high school and city park are located just east across Newcomb Street with two fully developed commercial shopping centers beyond, approximately one-quarter mile to the east.

**11. Other public agencies whose approval is required:** None

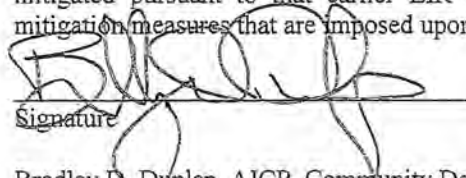
**ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:**

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- |   |   |   |
|---|---|---|
| <input type="checkbox"/> Aesthetics               | <input type="checkbox"/> Agriculture and Forestry Resources | <input type="checkbox"/> Air Quality                        |
| <input type="checkbox"/> Biological Resources     | <input type="checkbox"/> Cultural Resources                 | <input type="checkbox"/> Geology/Soils                      |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials      | <input type="checkbox"/> Hydrology/Water Quality            |
| <input type="checkbox"/> Land Use/Planning        | <input type="checkbox"/> Mineral Resources                  | <input type="checkbox"/> Noise                              |
| <input type="checkbox"/> Population/Housing       | <input type="checkbox"/> Public Services                    | <input type="checkbox"/> Recreation                         |
| <input type="checkbox"/> Transportation/Traffic   | <input type="checkbox"/> Utilities / Service Systems        | <input type="checkbox"/> Mandatory Findings of Significance |

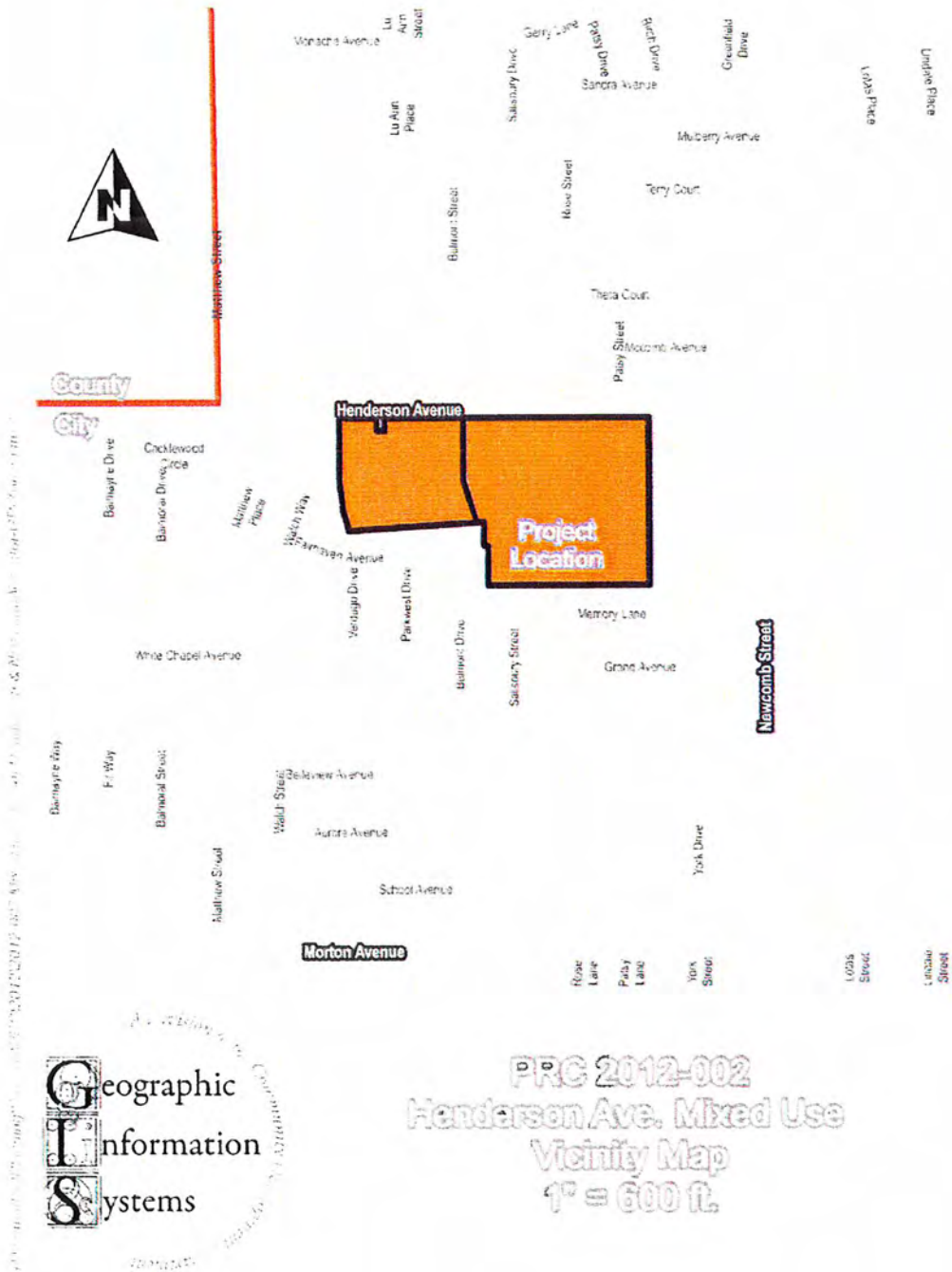
DETERMINATION: (To be completed by the Lead Agency)  
On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant impact" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

  
Signature

2/8/13  
Date

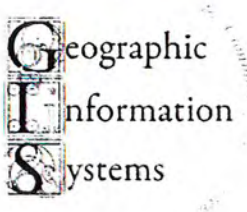
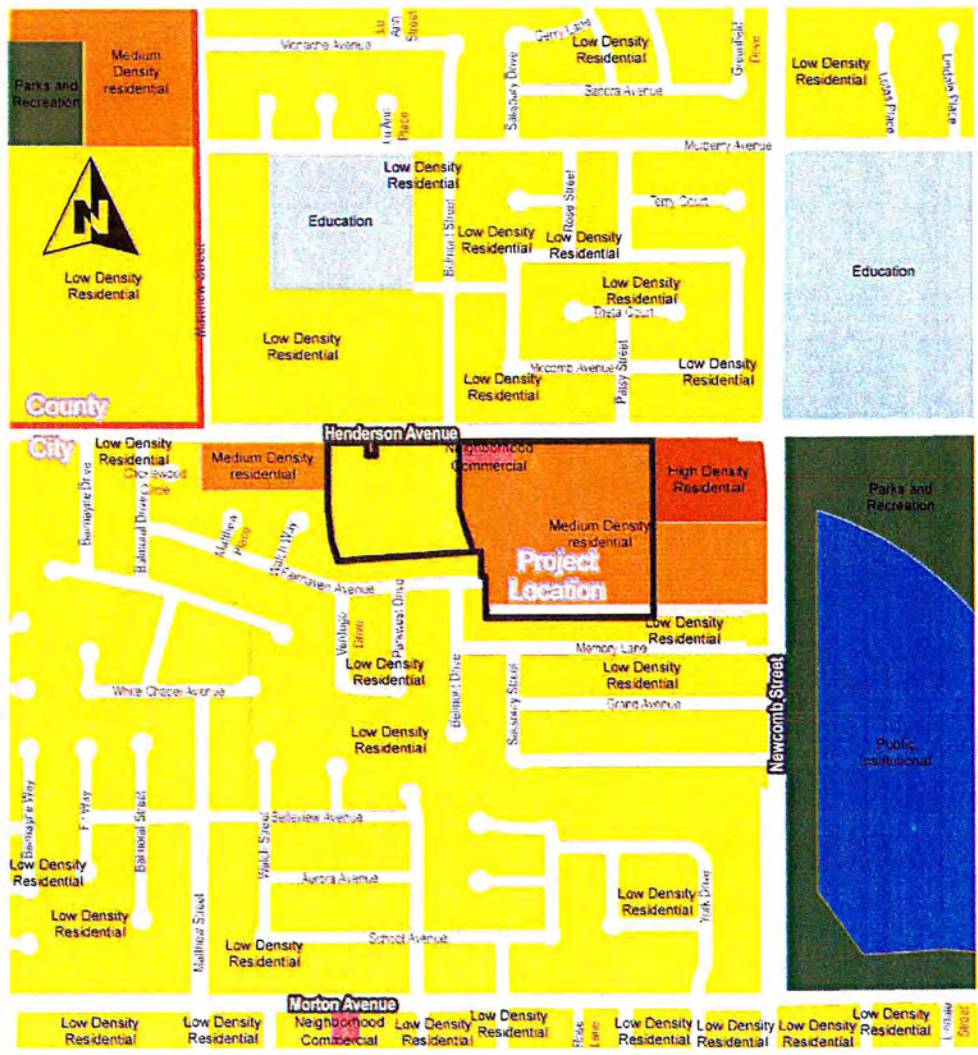
Bradley D. Danlap, AICP, Community Development Director  
Printed name



Geographic  
Information  
Systems

PRC 2012-002  
Henderson Ave. Mixed Use  
Vicinity Map  
1" = 600 ft.

**Figure 1**  
**Project Location**

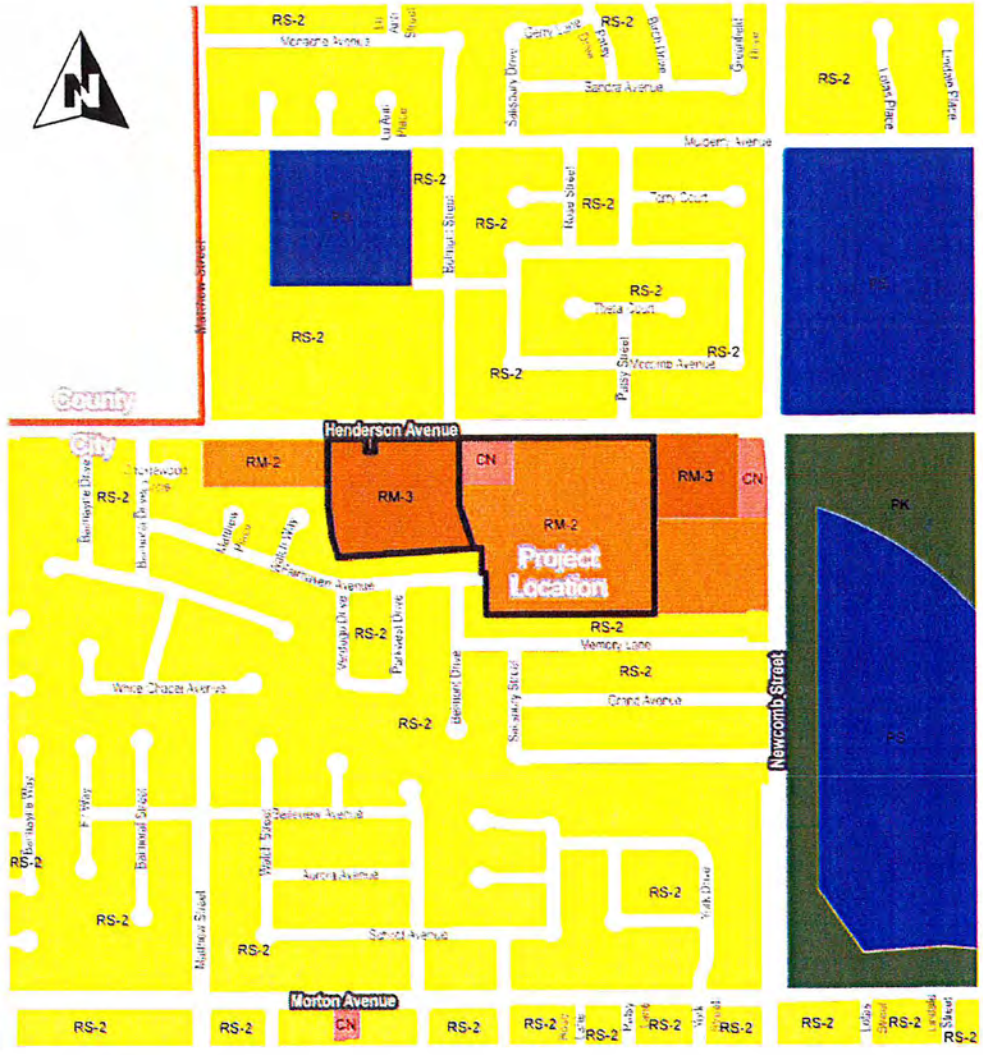


PRC 2012-002  
 Henderson Ave. Mixed Use  
 General Plan Land Use Map  
 1" = 600 ft.

**Figure 2**  
**Current General Plan Designation**







Geographic  
Information  
Systems

PRC 2012-002  
Henderson Ave. Mixed Use  
Zoning Map  
1" = 600 ft.

**Figure 4**  
**Current Zoning Designations**

Issues:

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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**I. AESTHETICS**

**Would the project:**

a) Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially degrade the existing visual character or quality of the site and its surroundings?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**Responses:**

**a, b) No Impact.** The project site is not located within or adjacent to a scenic vista. There would be no Impact.

**c) Less Than Significant Impact.** The proposed project site is undeveloped but surrounded by urban development including adjacent residential, school, park, and commercial uses within a quarter mile. There would be a less than significant impact.

**d) Less Than Significant Impact.** The proposed project would be subject to lighting standards found within the City of Porterville Development Ordinance. The Development Ordinance provides guidance for residential and commercial lighting to ensure that fixtures are shielded so as not to produce obtrusive glare onto the public right-of-way or adjoining properties (§300.07). There would be less than significant impact.

**II. AGRICULTURE AND FOREST RESOURCES**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Responses:**

**a) No Impact.** The proposed project site is vacant and zoned for residential and neighborhood commercial uses. The land has been undeveloped and underutilized for more than 25 years. There would be no impact to farmland nor conversion of existing farmland to any other use. While most soil types in the region could at some point have been deemed farmland conducive, the project's surrounding areas are primarily developed and at this time are considered urban built up land. Recent observations conclude that sites are disced primarily for the purposes of maintenance and weed abatement.

**b, c, d) No Impact.** The proposed project site is undeveloped and not currently zoned for agricultural uses. The site is not under any Williamson Act contract. The current zoning is high and medium density residential, and neighborhood commercial uses which do not accommodate agricultural uses.

**e) No Impact.** The proposed project is on the south side of Henderson Avenue. The nearest farmland use is approximately ¼ mile to the west on the north side of Henderson Avenue. That nearest agricultural use is surrounded on all sides by residential development.

**III. AIR QUALITY**

**Would the project:**

	Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**Response:**

**a) Less Than Significant Impact.** As none of the project components are considered a stationary source emitter, air quality impacts are limited to traffic volumes associated with the proposed development. A traffic analysis was prepared (Ruetters and Schuler, 2012) to identify potential impacts related to increased traffic volumes and project ingress & egress. The analysis ran a traffic model to determine average daily traffic (ADT) and level of service (LOS) analysis for roadway segments in the proximity of the proposed commercial mixed use project. Based on the results of the ADT analysis, the proposed project would result in less than significant impacts at the study roadway segments. The cumulative impacts associated with air quality of future traffic, with and without the project, results in all roadway intersections and segments operating at or above a LOS “D” thus resulting in a less than significant impact to transportation/circulation within Porterville as a result of the project.

The Project would not conflict with or obstruct the implementation of the air quality management standards. Standards set by the SJVAPCD, CARB, and Federal agencies relating to the Project would continue to apply. There would be less than significant impact.

**b) Less Than Significant Impact With Mitigation.** The San Joaquin Valley is designated as a Federal and State non-attainment area for O<sub>3</sub> and PM<sub>2.5</sub>. The SJVAPCD is the regional agency that regulates air permitting and maintains an extensive air quality monitoring network to measure criteria pollution concentrations throughout the San Joaquin Valley air basin. Compliance with the SJVAPCD Regulation VIII

guidelines would ensure potential impacts remain less than significant. A CalEEMod Model was conducted to estimate source emission levels. With exception to NOx, at 11.55 tpy (SJVAPCD 10 tpy threshold), the proposed development does not exceed the tonnage per year allowed for ROG, PM10, and PM2.5. The proposed project is self mitigating by nature of the City's General Plan and Development Ordinance. The City's policies require that commercial mixed use projects be designed to provide pedestrian/transit orientation, commit to bicycle parking racks, transit infrastructure and pedestrian improvements, enhancements, and access. Although the project proposes an emissions mitigating design concept, these elements and construction management elements shall be listed as mitigation measures within the mitigation monitoring program attached to the initial study checklist. Additionally, traditional residential wood fireplaces will be restricted (install of natural gas fireplaces or inserts shall be acceptable) as an additional mitigation measure.

**c) Less Than Significant Impact.** As discussed in Impact III-b, although the Project could result in the generation of criteria pollutants as a result of the proposed construction, compliance with the SJVAPCD Regulation VIII guidelines and the attached mitigation monitoring plan would ensure that potential impacts remain less than significant.

**d) Less Than Significant Impact.** Section 3 of the Guide for Assessing and Mitigating Air Quality Impacts defines a sensitive receptor as a location where human populations, especially children, seniors, and sick persons are present and where there is a reasonable expectation of human exposure to pollutants. Sensitive receptors normally refer to people with heightened sensitivity to localized, rather than regional pollutants. Compliance with the SJVAPCD Regulation VIII guidelines and mitigation monitoring plan would mitigate potential impacts to less than significant.

**e) Less than Significant Impact.** The Project would not be a source of odors because future construction of residential and commercial mixed uses would be consistent with existing and surrounding uses; therefore, there would be less than significant impact.

**IV. BIOLOGICAL RESOURCES**

**Would the project:**

	Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?  | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

**Response:**

**a, b, c, d, e, f) No Impact.** The site is currently vacant and undeveloped. The site is disked and cleaned annually for removal of debris and vegetation. Site visits and observations have not identified any trees, burrows, or channel waters for sensitive habitat or species within project area. Review of the City's General Plan Special Status Species & Sensitive Vegetation Figure 6-4 identifies the site as not an area of concern for sensitive habitat, species, or vegetation. The City does not have an adopted tree ordinance or habitat conservation plan.

**V. CULTURAL RESOURCES**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**Response:**

**a) Less Than Significant Impact.** The project proposes to develop approximately 23.4 acres of currently undeveloped land within the City of Porterville; As a result of prior uses, (walnut tree grove: farming and disking) the surface of the site has been significantly disturbed to a depth of at least eight feet. No known historic, archaeological, paleontological or geological resources exist on site. In the event that any as-yet undetected (i.e., buried) cultural resources are encountered on this property at a future time, work shall cease within a 50-foot area of the find, and a qualified archaeologist shall be contacted to evaluate any such discoveries. There would be less than significant impact.

**b) Less Than Significant Impact.** Any impacts to archaeological resources have been discussed in Impact V-a. There would be less than significant impact.

**c) Less Than Significant Impact.** No known paleontological resources exist within the Project area, nor are there any known geologic features in the Project areas. However, any impacts to paleontological or geologic resources have been discussed in V-a.

**d) Less Than Significant Impact.** No formal cemeteries or other places of human interment are known to exist within the project areas. However, in the event that any as-yet undetected (i.e., buried) cultural resources are encountered on this property at a future time of development, work shall cease within a 50-foot area of the find, and a qualified archaeologist shall be contacted to evaluate any such discoveries. Impacts would be less than significant.

**VI. GEOLOGY AND SOILS**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the most recently adopted Uniform Building Code creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Response:**

**a-i) No Impact.** According to the Department of Conservation, California Geological Survey, Special Publication 42, no faults occupy the County of Tulare. The two nearest faults are; first a small fault section related to the “Kern Front” located in Kern County approximately 25 miles to the south and west. The Second is the “Owens Valley” fault located north and east in Inyo County, approximately 50 miles away. The project is not in the vicinity of any Alquist-Priolo Earthquake Fault Zones as defined by the State Geologist, and would not expose people or structures to any seismic related impacts.



**a-ii). No Impact.** Impacts related to strong seismic ground shaking have been discussed in VI-a-i. There would be no impacts.

**a-iii). No Impact.** As discussed in VI-a-i., there are no fault zones within the project area and therefore there would be no seismic related ground failures or liquefaction.

**a-iv). No Impact.** The project area topography consists of flat land, comprised of stable soils and not subject to landslide. There would be no impact

**b). No Impact.** The future development of the site with commercial and residential mixed uses could result in minor ground disturbance through leveling, grading etc., there would be no substantial soil erosion or loss of topsoil. There would be no impact.

**c) No Impact.** The project site consists of sandy loam soils that are flat and fairly stable. Development of the site as proposed would result in minor ground disturbance through leveling, grading, etc. and could contribute to minor soil erosion during construction. Normal project procedures, including the enforcement of a site development plan and other development related conditions of approval would ensure the project area is not subjected to landslide, lateral spreading, subsidence, liquefaction or collapse.

**d) No Impact.** The Soil Survey of Tulare County, Central Part (Soil Survey) indicates that soils in the project area are not expansive. There would be no risk to life or property.

**e) No Impact.** The project does not include the use of septic tanks or other alternative waste water disposal systems. There would be no impact.

**VII. GREENHOUSE GAS EMISSIONS**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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- |  |                          |                          |                                     |                          |
|--|--------------------------|--------------------------|-------------------------------------|--------------------------|
| a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?      | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |
| b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

**Response:**

**a), b) Less Than Significant Impact.** While climate change has been a concern since at least 1988, as evidenced by the establishment of the United Nations and World Meteorological Organization’s Intergovernmental Panel on Climate Change (IPCC), the efforts devoted to greenhouse gas (GHG) emissions reduction and climate change research and policy have increased dramatically in recent years. In 2002, with the passage of Assembly Bill 1493 (AB 1493), California launched an innovative and pro-active approach to dealing with GHG emissions and climate change at the state level. AB 1493 requires the Air Resources Board (ARB) to develop and implement regulations to reduce automobile and light truck GHG emissions; these regulations applied to automobiles and light trucks beginning with the 2009 model year.

On June 1, 2005, Governor Arnold Schwarzenegger signed Executive Order S-3-05. The goal of this Executive Order is to reduce California’s GHG emissions to: 1) 2000 levels by 2010, 2) 1990 levels by the year 2020, and 3) 80% below the 1990 levels by the year 2050. In 2006, this goal was further reinforced with the passage of Assembly Bill 32 (AB 32), the Global Warming Solutions Act of 2006. AB 32 sets the same overall GHG emissions reduction goals while further mandating that ARB create a plan, which includes market mechanisms, and implement rules to achieve “real, quantifiable, cost-effective reductions of greenhouse gases.” Executive Order S-20-06 further directs state agencies to begin implementing AB 32, including the recommendations made by the state’s Climate Action Team.

Climate change and GHG reduction is also a concern at the federal level; however, at this time, no legislation or regulations have been enacted specifically addressing GHG emissions reductions and climate change. Compliance with AB1493 and the San Joaquin Valley Air Resource Board development requirements would ensure future potential impacts to less than significant.

**VIII.HAZARDS/HAZARDOUS MATERIALS**

**Would the project:**

	Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Response:**

**a) No Impact.** The proposed project uses would not require transport, use or disposal of hazardous materials. There would be no impact.

**b) No Impact.** The Project would not create a significant hazard to the public or the environment as the Project would not discharge hazardous materials into the environment. There would be no impact.

**c) No Impact.** The proposed project will have no effect on hazardous emissions, involve hazardous materials, or create a hazard to adjacent schools in any way. There would be no impact.

**d) No Impact.** The Project does not involve land that is listed as an active hazardous materials site pursuant to Government Code Section 65962.5 and is not included on the list compiled by the Department of Toxic Substances Control. There would be no impact.

**e) No Impact.** The nearest airport, the Porterville Municipal Airport, is approximately 3.2 miles south of the project area. Due to the project description and the distance to the airport, there would be no impact.

**f) No Impact.** Any impacts regarding private airstrips have been discussed in Impact VII-e. There would be no impact.

**g) No Impact.** The Project does propose changes to any publicly accessed routes, and would not interfere with implementation of an emergency response plan or evacuation. There would be no impact.

**h) No Impact.** The proposed project area is currently undeveloped but the entire surrounding is developed with residential, school, park and commercial uses; the project site has been disked annually. The project would not result in risk of loss, injury, or death involving wildland fires.

**VIV. HYDROLOGY AND WATER QUALITY**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Responses:**

**a) Less Than Significant Impact.** The project area is not within or adjacent to any large water body although the Porter Slough does run along the northern property lines. Along Henderson Avenue, the Porter Slough has previously been encased within a 36in concrete pipe. The project is within the City's Master Plan for Storm Drainage. Consequently, the storm water generated from future development has been anticipated. The Project would not violate any water quality standards and would not impact waste discharge requirements. The impact would be less than significant.

**b) Less Than Significant Impact.** The Project site is located in the Tulare Lake Basin, an area considered to be in a state of overdraft according to the City's Urban Water Management Plan 2007. General Plan Section 8.1 "Water Supply and Conservation," provides current and planned water supplies and includes a schedule for meeting future demand including the increased potential units associated with the project. The General Plan also identifies programs and policies for water conservation and planned supply to service demand through 2030 including an established goal of reducing the per capita water demand by 10% per day by 2030. As the County continues to review and finalize the City's Urban Development Boundaries, the City anticipates updating and completing the Water System Master Plan by 2014-2015. The update plans would include a water balance study and analysis of the distribution facilities needed to accommodate population growth.

Development in the project area has been anticipated and water demands would not challenge the City's supply. City services outline and would serve the entire project area as development occurs. The impact would be less than significant.

**c) Less Than Significant Impact.** Drainage patterns would not change substantially as a result of the proposed project. As development occurs, curb and gutter improvements would improve the management of stormwater flows to reduce existing erosion or siltation. No modifications to natural or created channels would occur, as there are none within the project area. As a part of the development, implementation of erosion control measures described by the Tulare County Development Standards and mandated in the Stormwater Pollution Prevention Program would minimize any potential impacts to less than significant.

**d) Less Than Significant Impact.** Any impacts regarding the alteration of drainage patterns to increase runoff water that would potentially induce flooding have been discussed in the impact analysis for Impact VIV-c.

**e) Less Than Significant Impact.** Any impacts regarding the creation or contribution to runoff water that would potentially exceed the capacity of existing stormwater drainage systems have been discussed in the impact analysis for Impact VIV-c.

**f) Less Than Significant Impact.** Any impacts to water quality have been discussed in the impact analysis for Impact VIV-a.

**g) Less than Significant Impact.** According to the Federal Emergency Management Agency (FEMA) National Flood Insurance Program (NFIP) Flood Insurance Rate Maps (FIRM) 06107C1633E dated June 16, 2009, the project areas is are within Zone X, determined to be outside the 2% annual chance floodplain. There would be less than significant impact with regard to flood related events.

**h) No Impact.** Any impacts regarding the placement of structures in a 100-year flood hazard area that would impede or redirect flood flows have been discussed in the analysis of Impact VIV-g.

**i) No Impact.** The dam potentially affecting the Project, Success Dam, is approximately 10 miles to the east of the Project site. According to Tulare County's Geographic Information Systems data, the Project area is not located within the 24-hour inundation area of the Success dam in the event of its failure.

**j) No Impact.** The nearest large body of water is Lake Success, which is located approximately 10 miles to the east of the Project site. Due to the lengthy distance between the lake and the Project site, there would be no potential for seiche or tsunami to occur. There would be no impact.

**X. LAND USE AND PLANNING**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the General Plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Conflict with any applicable habitat conservation plan or natural community conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Response:**

**a) No Impact.** The project site is undeveloped. Upon development, the residential and commercial mixed uses would be fully integrated into their surrounding urban area. The proposed project would create opportunity for a mixed use development to bring additional services and retail to the surrounding community.

**b) Less Than Significant Impact.** The proposed residential and mixed use project includes a General Plan amendment and Zone Change that would assure that the Commercial Mixed Use project conforms to the adopted City of Porterville General Plan and Development Ordinance.

**c) No Impact.** No habitat conservation plan or natural community conservation plan has been adopted for the project areas.



**XI. MINERAL RESOURCES**

**Would the project:**

	Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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- a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?
- b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Response:**

**a) No Impact.** The Porterville General Plan (2008) includes Figure 6-3 Soil and Mineral Conservation, which indicates the locations of State-designated Mineral Resource Zones or areas possessing minerals which are of state-wide or regional significance. According to the map, none the project area is not located in a Mineral Resource Zone. Therefore, the Project would not result in the loss of an available known mineral resource. There would be no impact.

**b) No Impact.** The Project site is not delineated on a local land use plan as a locally important mineral resource recovery site; therefore, the existence of the Project would not result in the loss of availability of any mineral resources. There would be no impact.

**XII. NOISE**

**Would the project result in:**

	Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Response:**

**a), c), d) Less Than Significant Impact.** The project site is undeveloped but if approved, standard noise requirements shall be implemented per the Development Ordinance and Chapter 18, Article XI of the Municipal Code. The project would not significantly change the noise level generation in the project areas. Conversely, as these sites develop into residential and commercial uses, they would be subject to the City’s Noise Ordinance, providing greater enforcement mechanisms to monitor and reduce noise generation during hours of construction once developed. Impacts would be less than significant.

**b) Less Than Significant Impact.** The City of Porterville has set forth vibration guidelines described in the Development Ordinance Section 307.06, which states that “no vibration shall be produced that is discernable without the aid of instruments by a reasonable person at the lot lines of the site. Vibration from temporary construction, demolition, and vehicles that enter and leave the subject parcel (e.g., construction, equipment, trains, trucks, etc.) are exempt from this standard.” Impacts would be less than significant.

**e) No Impact.** The Porterville General Plan (2008), indicates in Figure 9-3, Projected Noise Contours 2030, that the Project site is located more than three two miles outside of the airport 55 dB CNEL noise contour. There would be no impact.

**f) No Impact.** There are no private airstrips in the project vicinity. There would be no impact.

**XIII. POPULATION AND HOUSING**

**Would the project:**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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- |   |                          |                          |                                     |                                     |
|---|--------------------------|--------------------------|-------------------------------------|-------------------------------------|
| a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |
| b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?   | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |

**Response:**

**a) Less Than Significant Impact.** The proposed 23.4± acre project area has been identified and planned on the City's General Plan high and medium density residential uses, and for neighborhood commercial. The proposed General Plan Amendment and Zone Change would provide for the entire 23.4± acres to be developed as commercial mixed uses. The General Plan assures consistency with zoning and provides guiding and implementation policies to assure the City is capable of meeting population and housing demands. The subject site is included within the City of Porterville Sewer, Water, and Storm Drain Master Plans. Roads and other infrastructure are found along Henderson Avenue and Newcomb Street frontages. including city sewer, water, and storm drain systems to meet current and future demands. The impacts to population and housing would be less than significant.

**b), c) No Impact.** The proposed project site is undeveloped and has not been in use for over 25 years. No existing uses would be displaced by the project.

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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**XIV. PUBLIC SERVICES**

a) Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Other Public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**Response:**

a) The site has been planned for high and medium density residential and neighborhood commercial uses. The proposed multi-family and commercial mixed use project is being evaluated for impacts to City services.

**Less Than Significant Impact: Fire Protection** – The proposed project is consistent with planned growth in the city, and would not result in increased in service times for safety response. The impact would be less than significant impact.

**Less Than Significant Impact: Police Protection** – The proposed project is consistent with planned growth in the city, and would not result in increased in service times for safety response. The impact would be less than significant impact.

**Less Than Significant Impact: Schools** – The project site is located within the Burton and Porterville Unified School District; Monache High School is located immediately to the east across Newcomb Street and on the north side of Henderson Avenue and within the project area. Additional students would slowly be introduced at the time of development within School Districts. The Project would result in an increase of population that would have a less than significant impact.

**Less Than Significant Impact: Parks** – As the Project introduces some population growth to the area , the project would not create a need for additional park or recreational services. The project proposes five recreation areas including a recreation building, two open spaces and two swimming pool areas. Residents within the Project areas are already able to take advantage of the City’s numerous recreational areas including Veteran’s Park across Newcomb Street and the high school north of the park. There would be less than significant impact.

**No Impact: Other public facilities** – As the Project would slowly introduce population growth, the project would not create additional need for other public facilities. There would be less than significant impact.

**XV. RECREATION**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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- |  |                          |                          |                          |                                     |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?                        | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

**Response:**

**a) No Impact.** The project has been evaluated for impacts to neighborhood and regional parks or other recreational facilities or the need for these additional facilities.

Currently the City has 15 parks for a total of almost 295 acres of parkland, plus other community facilities. The City's Park Standard for neighborhood and community parks is 5.0 acres per 1,000 residents. The goal for all parkland including specialized facilities, is 10.0 per 1000 residents. The General Plan provides approximately 870 acres of parkland within the Planning Area. Buildout of the proposed General Plan would result in an approximate park ratio of 6.3 acres of neighborhood and community parks and 10.3 acres of total parkland per 1,000 residents based on the General Plans 2030 planned 107,300 population.

**b) No Impact.** The project proposes five recreation areas including two (2) recreation buildings (1,344 sqft. & 3,300sqft.), two open spaces ( 14,000± sqft. and 18,000± sqft.) and two swimming pool areas (each 18,000± sqft.).

**XVI. TRANSPORTATION/TRAFFIC**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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**Would the project:**

a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Response:**

A traffic analysis was prepared by Ruetgers and Schuler (2012) in order evaluate the potential traffic impacts as a result of the construction of the proposed mixed use development in the City of Porterville. Intersection and roadway capacity was analyzed for 11 intersections in the vicinity of the project for weekday AM and PM peak hours. Of the 11 intersections, all currently operate at or above a LOS C. In addition to intersection

analysis, the driveway operation and configuration along Henderson Avenue was reviewed with recommendations for number and location of driveways. The intersections evaluated were:

- Westfield Avenue at: Newcomb Street and Prospect Street
- Mulberry Avenue at: Newcomb Street and Prospect Street
- Henderson Avenue at: Westwood Street, Mathew Street, Newcomb Street, Prospect Street, State Route 65 southbound lane ramps, State Route 65 northbound lane ramps, and Porter Road.

**a) Less Than Significant Impact.**

The analysis of existing traffic conditions at 11 intersections within the study area resulted in operation at or above LOS C. In addition to intersection analysis, the driveway operation and configuration along Henderson Avenue was reviewed with recommendations for number and location of driveways.

**b) Less Than Significant Impact with Mitigation Incorporation.** Future cumulative analysis, with and without the project, results in all roadway intersections and segments operating at or above a Level of Service (LOS) D. However, the queue length for one intersection exceeded the existing capacity. Therefore, mitigation was found necessary for this intersection in the future year 2030.

The eastbound left turn storage lane at the intersection of Henderson Avenue and Newcomb Street would need to be lengthened to 250 feet to meet future queue length requirements. The proposed project's share of the required improvements was calculated to be 52.71%. The installation of the improvements, with a reimbursement plan for the remaining amount, would be addressed as a condition of project approval and would mitigate the potential impact to less than significant.

**c) No Impact.** The Project is located approximately three miles northeast of the Porterville Municipal Airport. The Project would not cause an increase in air traffic levels or cause a change in air traffic location. There would be no impact.

**d) No Impact.** Roadway design features have been specifically coordinated with public safety agencies and the City's Public Works Department to include design features that would reduce hazards and the proposed changes to land uses would not result in a use incompatible to existing surrounding uses. There would be no impact.

**e) No Impact.** Circulation would not be modified as a result of this Project; therefore, there would be no impact to any emergency access.

**f) No Impact.** There are no adopted alternative transportation policies, plans, or programs in the Project area. There would be no impact.



**XVII. UTILITIES AND SERVICE SYSTEMS**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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**Would the project:**

a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**Response:**

**a) Less Than Significant Impact.** The capacity of the City of Porterville Wastewater Treatment Plant is currently eight million gallons per day. Current usage averages 5.2 million gallons per day. The 2001 Sewer Master Plan addresses the City's sewage collection system capacity and operational needs and recommends a long-term capital improvement program. If the proposed project is approved, development would be permitted

with discretionary approvals allowing for conditions to be placed accordingly. The impact would be less than significant.

**b) Less Than Significant Impact.** The project will not require expansion or construction of new facilities. The General Plan provides guiding and implementation policies to assure the City is capable of meeting increased demands. The subject site is included within the City of Porterville Sewer, Water, and Storm Drain Master Plans. As development and infill-development occurs throughout the city, the sewer, water, and storm drain systems would be extended as necessary to accommodate new construction. Funding mechanisms are identified in the Master Plans to ensure that the expansion of each system keeps pace with development. The impact would be less than significant.

**c) Less Than Significant Impact.** Any impacts regarding the need for storm water drainage facilities have been discussed in Impact XVII-b. The impact would be less than significant.

**d) Less Than Significant Impact.** Development of the project area would not challenge the City's supply. General Plan Section 8.1 "Water Supply and Conservation," provides current and planned water supplies and includes a schedule for meeting future demand including the increased potential units associated with the project. The General Plan also identifies programs and policies for water conservation and planned supply to service demand through 2030 including an established goal of reducing the per capita water demand by 10% per day by 2030. As the County continues to review and finalize the City's Urban Development Boundaries, the City anticipates updating and completing the Water System Master Plan by 2014-2015. The updates plan would include a water balance study and analysis of the distribution facilities needed to accommodate population growth. City services outline and would serve the entire project site as development occurs. The impact would be less than significant.

**e) Less Than Significant Impact.** Any impacts regarding wastewater treatment have been discussed in Impact XVI-a. The impact would be less than significant.

**f) Less Than Significant Impact.** The project area is undeveloped but is in an area that currently receives solid waste services from the City of Porterville. Both the private and public solid waste services make use of the Teapot Dome Landfill operated by the County of Tulare. Adequate capacity exists in that landfill for potential development consistent with the proposed High Density Residential Zoning. The impact would be less than significant.

**g) Less Than Significant Impact.** Any impacts regarding solid waste have been discussed in Impact XVII-f. The impact would be less than significant.

**XVII. MANDATORY FINDINGS OF SIGNIFICANCE**

Potentially Significant Impact	Less than Significant With Mitigation Incorporation	Less than Significant Impact	No Impact
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- |  |                          |                                     |                                     |                                     |
|--|--------------------------|-------------------------------------|-------------------------------------|-------------------------------------|
| a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? | <input type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/>            | <input checked="" type="checkbox"/> |
| b) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects)?   | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/>            | <input type="checkbox"/>            |
| c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?  | <input type="checkbox"/> | <input type="checkbox"/>            | <input checked="" type="checkbox"/> | <input type="checkbox"/>            |

**Response:**

a) **No Impact.** The analysis conducted in this Initial Study/Mitigated Negative Declaration results in a determination that the Project would have no negative effect on the local natural environment. Although the project area is undeveloped, the site is disked annually for weed control and abatement. The project area is substantially urban and would not result in loss of native habitat. The potential for impacts to biological and cultural resources from the project would be less than significant as discussed in the previous impact sections. Accordingly, the project would involve no potential for significant impacts through the degradation of the quality of the environment, the reduction in the habitat or population of fish or wildlife, including endangered plants or animals, the elimination of a plant or animal community or example of a major period of California history or prehistory. There would be no impact.

b) **Less Than Significant Impact with Mitigation Incorporation.** As discussed within the preceding impact analyses, the proposed project has potential for traffic and air related impacts. Those impacts have been addressed with mitigation measures, which when implemented will ensure not only the project related but the cumulative impact of the project. Compliance with applicable codes, ordinances, laws and other required regulations would further assure that potential impacts associated with development applications remain at a less than significant level.

**c) Less Than Significant Impact.** The Project would not result in substantial adverse effects on human beings, either directly or indirectly. Adverse effects on human beings resulting from implementation of the project would be less than significant.

## References

- Department of Conservation, California Geological Survey. Special Publication 42- Fault Rupture Hazard Zones in California. Table 4. May 1999.
- California Air Pollution Control Officers Association. CEQA and Climate Change, January 2008.
- City of Porterville, 2030 General Plan. March 4, 2008.
- City of Porterville, Development Ordinance. June 4, 2010.
- City of Porterville, Noise Ordinance, Ordinance
- City of Porterville, Sewer System Master Plan, February 2001
- City of Porterville, Water System Master Plan, February 2001
- Federal Emergency Management Agency. Flood Insurance Rate Map. Map Number 06107C1630E through 06107C16362E. June 16, 2009.
- Guide for Assessing and Mitigating Air Quality Impacts. San Joaquin Valley Air Pollution Control District. January 2002.
- San Joaquin Valley Air Pollution Control District, Regulation VIII
- United States Department of Agriculture, Natural Resources Conservation Service. Soil Survey of Tulare County, Central Part.

## MITIGATION MONITORING AND REPORTING PROGRAM

Impacts	Mitigation Measures	Implementation	Monitoring	Time Span
Air Quality	<p><b>Mitigation Measure #III a), b), c), d):</b>            Construction contracts shall require the primary construction contractor to implement the following practices during all construction activities:</p> <ul style="list-style-type: none"> <li>▪ Construction equipment shall use aqueous diesel fuel and shall be equipped with particulate traps and catalytic converters.</li> <li>▪ All disturbed areas, including soil piles, areas that have been graded, and unpaved roads shall be watered twice daily and, when feasible, covered or enclosed.</li> <li>▪ When materials are transported offsite, loads shall be wetted and covered securely and at least two feet of freeboard shall be maintained.</li> <li>▪ Limit traffic speeds on unpaved roads to 15 mph and install sandbags or other erosion control measures to prevent silt runoff to public roadways from sites with a slope greater than one percent.</li> <li>▪ Install wheel washers for all exiting trucks, or wash off all trucks and equipment leaving the site.</li> <li>▪ Turn off equipment not in use for more than ten minutes.</li> <li>▪ Curtail construction activities when the County's Air Quality Index exceeds 150.</li> <li>▪ Traditional residential wood fireplaces will be restricted (install of natural gas fireplaces or inserts shall be acceptable)</li> </ul>	<p>Condition of Building Permit, to be the responsibility of the applicant and applicant's contractor. To comply with the District's restriction of PM<sub>10</sub> generating activities, the project proponent will follow all Regulation VIII requirements (Tables 1 and 2). In addition, the proponent will further mitigate impacts by meeting the enhanced and additional control measures for construction emissions of PM<sub>10</sub> (Table 3). These actions will reduce any potential impact to less than significant</p>	<p>City of Porterville Building Department to verify at plan check and the San Joaquin Valley Air Pollution Control District will be notified if necessary during construction.</p>	<p>Beginning with construction permit and terminating with issuance of Notice of Completion</p>

Impacts	Mitigation Measures	Implementation	Monitoring	Time Span
<b>Transportation/Traffic</b>	<b>Mitigation Measure #XV a):</b> Mitigation measures for the Henderson MU-V Commercial Mix Use Development; The developer/applicant shall dedicate right of way to the City for construction of the driveway transitional lanes, including sidewalks, as shown on the revised site plan and per the mitigation measures described in the Traffic Study prepared by Ruetters & Schuler Civil Engineers. Dedications shall include the ADA compliant curb returns at each driveway.  Off-site median islands shall comply with the traffic study prepared by Ruetters and Schuler Civil Engineers.	Condition of Building Permit, to be the responsibility of the applicant and applicant's contractor	City of Porterville Planning and Building Department to verify at plan check.  City of Porterville Building Inspector during construction	Completed at time of final building inspection.

RESOLUTION NO. \_\_\_\_\_

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF PORTERVILLE  
CONTAINING FINDINGS IN SUPPORT OF APPROVAL OF  
A MITIGATED NEGATIVE DECLARATION OF ENVIRONMENTAL IMPACT  
FOR THE PACIFIC RIM COMMERCIAL MIXED USE PROJECT

WHEREAS: The City Council of the City of Porterville at its regularly scheduled meeting of March 5, 2013, conducted a public hearing to consider entitlements needed to permit the proposed Pacific Rim Commercial Mixed Use Project (2012-002) including a General Plan Amendment, Zone Change, Tentative Parcel Map, and Conditional Use Permit; and

WHEREAS: General Plan Amendment 2012-002-G proposes to change the land use designation on the General Plan Land Use Diagram for the subject site from Medium-Density Residential, High-Density Residential, and Neighborhood Commercial to Commercial Mixed Use; and

WHEREAS: Zone Change 2012-002-Z proposes to change the present zoning classifications of the subject parcels from Medium-Density Residential, High-Density Residential, and Neighborhood Commercial to Commercial Mixed Use; and

WHEREAS: Tentative Parcel Map 2012-002-P proposes to divide a 23.4± acre site into four parcels: Parcel 1- 5.70± acres; Parcel 2- 4.49± acres; Parcel 3- 1.57± acres; and Parcel 4- 11.69± acres; and

WHEREAS: Conditional Use Permit 2012-002-C proposes to accommodate personal storage uses in the CMX zone; and

WHEREAS: The City Council considered the following findings in its review of the environmental circumstances for this project:

1. That a Mitigated Negative Declaration was prepared in accordance with the California Environmental Quality Act.
2. That the subject project will not create adverse environmental impacts. The project was evaluated in light of the prepared environmental initial study; one comment was received from an interested party during the review period. In light of the record and information received, it was determined that potential impacts associated with the proposed project would be less than significant.
3. That the City Council is the decision making body for the project.
4. On February 5, 2013, the environmental coordinator made a preliminary determination that a Mitigated Negative Declaration would be appropriate for the proposed project. The Initial Study has been transmitted to interested agencies and groups for a twenty (20) day review period from February 8, 2013, to February 28, 2013. One comment was received from the Porterville Unified School District related to vehicular and pedestrian traffic.



5. That review of the environmental circumstances regarding this project indicates that no adverse impacts would accrue to wildlife resources from implementation of this project. An on-site inspection was conducted, and a traffic study prepared to consider the impacts of the project on the environment. No natural resource concerns were identified with the proposed project. However, traffic impacts were identified. Mitigation measures to reduce impacts to less than significant were defined and have been incorporated into the Mitigation Monitoring Program attached hereto as Exhibit A.
6. That the environmental assessment and analysis prepared for this project supporting the Mitigated Negative Declaration reflects the independent judgment of the City of Porterville.
7. The developer/applicant shall comply with all mitigation measures adopted as a component of the approval of the Mitigated Negative Declaration for this project. The developer/applicant will be required to sign a document committing to comply with the adopted mitigation measures prior to any construction on the site.

NOW, THEREFORE, BE IT RESOLVED: That the City Council of the City of Porterville does hereby approve the Mitigated Negative Declaration prepared for the General Plan Amendment 2012-002-G, Zone Change 2012-002-Z and related development of the Pacific Rim Commercial Mixed Use Project, and that the mitigation measures defined in Exhibit A shall be implemented by the developer/applicant or his/her partners and successors with project implementation.

By: \_\_\_\_\_  
Virginia R. Gurrola, Mayor

ATTEST:  
John D. Lollis, City Clerk

By: \_\_\_\_\_  
Patrice Hildreth, Chief Deputy City Clerk

## MITIGATION MONITORING AND REPORTING PROGRAM

Impacts	Mitigation Measures	Implementation	Monitoring	Time Span
Air Quality	<p>Mitigation Measure #III a), b), c), d): Construction contracts shall require the primary construction contractor to implement the following practices during all construction activities:</p> <ul style="list-style-type: none"> <li>▪ Construction equipment shall use aqueous diesel fuel and shall be equipped with particulate traps and catalytic converters.</li> <li>▪ All disturbed areas, including soil piles, areas that have been graded, and unpaved roads shall be watered twice daily and, when feasible, covered or enclosed.</li> <li>▪ When materials are transported offsite, loads shall be wetted and covered securely and at least two feet of freeboard shall be maintained.</li> <li>▪ Limit traffic speeds on unpaved roads to 15 mph and install sandbags or other erosion control measures to prevent silt runoff to public roadways from sites with a slope greater than one percent.</li> <li>▪ Install wheel washers for all exiting trucks, or wash off all trucks and equipment leaving the site.</li> <li>▪ Turn off equipment not in use for more than ten minutes.</li> <li>▪ Curtail construction activities when the County's Air Quality Index exceeds 150.</li> <li>▪ Traditional residential wood fireplaces will be restricted (install of natural gas fireplaces or inserts shall be acceptable)</li> </ul>	<p>Completion of Building Permit, to be the responsibility of the applicant and applicant's contractor. To comply with the District's restriction of PM<sub>10</sub> generating activities, the project proponent will follow all Regulation VIII requirements (Tables 1 and 2). In addition, the proponent will further mitigate impacts by meeting the enhanced and additional control measures for construction emissions of PM<sub>10</sub> (Table 3). These actions will reduce any potential impact to less than significant</p>	<p>City of Porterville Building Department to verify at plan check and the San Joaquin Valley Air Pollution Control District will be notified if necessary during construction.</p>	<p>Beginning with construction permit and terminating with issuance of Notice of Completion</p>

EXHIBIT A

Impacts	Mitigation Measures	Implementation	Monitoring	Time Span
Transportation/Traffic	<p data-bbox="251 325 284 735"><b>Mitigation Measure #XV a):</b> Mitigation measures for the Henderson MU-V Commercial Mix Use Development; The developer/applicant shall dedicate right of way to the City for construction of the driveway transitional lanes, including sidewalks, as shown on the revised site plan and per the mitigation measures described in the Traffic Study prepared by Ruetters &amp; Schuler Civil Engineers. Dedications shall include the ADA compliant curb returns at each driveway.</p> <p data-bbox="251 735 284 1155">Off-site median islands shall comply with the traffic study prepared by Ruetters and Schuler Civil Engineers.</p>	Condition of Building Permit, to be the responsibility of the applicant and applicant's contractor	City of Porterville Planning and Building Department to verify at plan check.  City of Porterville Building Inspector during construction	Completed at time of final building inspection.

RESOLUTION NO. \_\_\_\_\_

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF PORTERVILLE  
CONTAINING FINDINGS IN SUPPORT OF APPROVAL OF GENERAL PLAN  
AMENDMENT 2012-002-G FOR THE PROPOSED PACIFIC RIM COMMERCIAL  
MIXED USE PROJECT LOCATED GENERALLY AT THE SOUTHWEST CORNER OF  
HENDERSON AVENUE AND NEWCOMB STREET

WHEREAS: The City Council of the City of Porterville at its regularly scheduled meeting of March 5, 2013, conducted a public hearing to consider approval of a General Plan amendment from High Density Residential, Medium Density Residential, and Neighborhood Commercial to Commercial Mixed Use for a 23.4± acre parcel located on the south side of Henderson Avenue, west of Newcomb Street (APN 245-410-032); and

WHEREAS: The project site is currently undeveloped; and

WHEREAS: On February 5, 2013, the Environmental Coordinator made a preliminary determination that a Mitigated Negative Declaration would be appropriate for the proposed project; and

WHEREAS: Approval of the General Plan Amendment and Zone Change would allow the project site to be developed with multiple uses in an integrated manner, and provide residential, office, commercial, and storage uses to the existing neighborhood; and

WHEREAS: Approval of the Commercial Mixed Use Project furthers the goals and objectives of the General Plan by developing higher density residential uses in close proximity to services, such as schools, parks and commercial businesses; and

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Porterville does hereby make the following findings:

1. The proposed General Plan Amendment has been requested by the applicant to accommodate opportunities for a commercial mixed use development that was unanticipated at the time of the adoption of the General Plan. The proposed Commercial Mixed Use Designation modifies the existing land use plan by integrating the various components of the development into a unified development. In essence, the existing land use designations would allow for multi-family residential and commercial development but in different proportions. The proposed mix of uses facilitated through the approval of the requested amendment are complementary and mutually supportive.
2. Based on review of application materials and submitted plans, the proposed project serves to fulfill the goals of the General Plan as adopted, and the amendment of the land use designation on the subject parcel (APN 245-410-032)

**ATTACHMENT  
ITEM NO. 4**

does not infringe on the goals of the General Plan to maintain transitions between types and intensities of land use.

3. That a Negative Declaration was prepared for the project in accordance with the California Environmental Quality Act and was transmitted to interested agencies and made available for public review and comment. The review period ran for twenty (20) days, from February 8, 2013, to February 28, 2013.
4. The City Council is the decision-making body for the project.

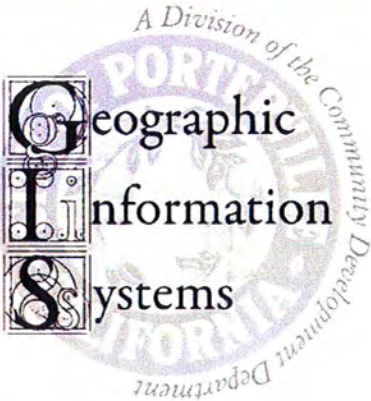
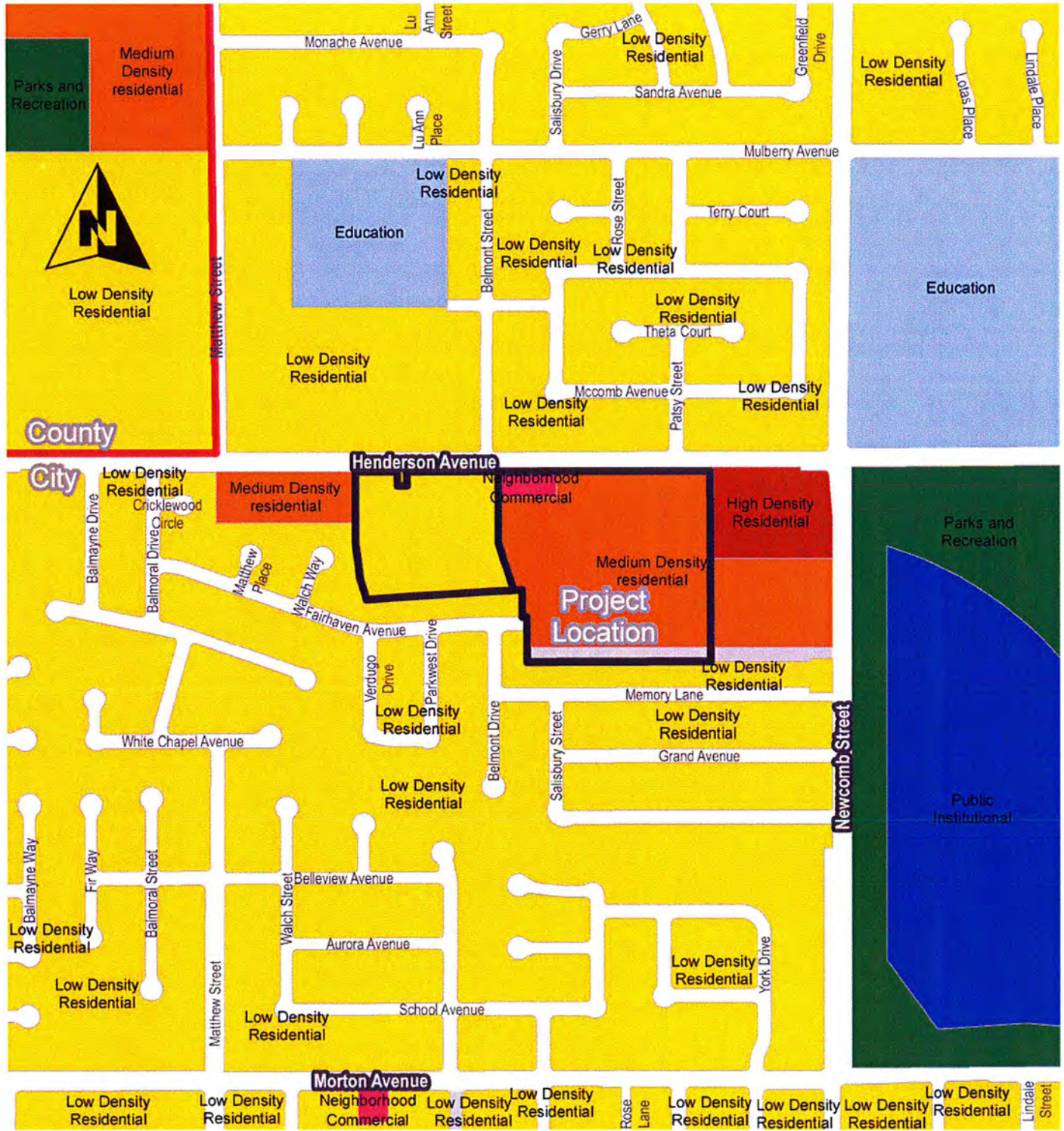
BE IT FURTHER RESOLVED: That the City Council does hereby approve the General Plan Amendment from High Density Residential, Medium Density Residential, and Neighborhood Commercial to Commercial Mixed Use for development as represented and incorporated herein as Exhibit A.

PASSED, APPROVED AND ADOPTED this 5<sup>th</sup> day of March 2013.

By: \_\_\_\_\_  
Virginia R. Gurrola, Mayor

ATTEST:  
John D. Lollis, City Clerk

By: \_\_\_\_\_  
Patrice Hildreth, Chief Deputy City Clerk



PRC 2012-002  
Henderson Ave. Mixed Use  
General Plan Land Use Map  
1" = 600 ft.

EXHIBIT A

ORDINANCE NO. \_\_\_\_\_

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF PORTERVILLE  
APPROVING ZONE CHANGE 2012-002-Z

FROM RM-2 (MEDIUM DENSITY RESIDENTIAL), RM-3 (HIGH DENSITY RESIDENTIAL), AND CN (NEIGHBORHOOD COMMERCIAL) TO CMX (COMMERCIAL MIXED USE) FOR THAT 23.4± ACRE SITE LOCATED GENERALLY AT THE SOUTHWEST CORNER OF HENDERSON AVENUE AND NEWCOMB STREET

WHEREAS: The City Council of the City of Porterville at its regularly scheduled meeting of March 5, 2013, conducted a public hearing to approve findings and consider Zone Change 2012-002-Z, being a change of zone from RM-3 (High Density Residential), RM-2 (Medium Density Residential), and CN (Neighborhood Commercial) to CMX (Commercial Mixed Use) for the site located on the south side of Henderson Avenue, west of Newcomb Street (APN 245-410-032); and

WHEREAS: The City Council of the City of Porterville determined that the proposed Zone Change (2012-002-Z) is consistent with the guiding and implementation policies of the adopted 2030 General Plan; and

WHEREAS: That a Mitigated Negative Declaration was prepared for the project in accordance with the California Environmental Quality Act and was transmitted to interested agencies and made available for public review and comment. The review period ran for twenty (20) days from February 8, 2013 to February 28, 2013; and

WHEREAS: The City Council made the following findings that the proposed project will advance the goals and objectives of and is consistent with the policies of the General Plan and any other applicable plan that the City has adopted.

- a. The project supports and complies with the following General Plan guiding policies:
  - LU-G-1: Promote a sustainable, balanced land use pattern that responds to existing needs and future needs of the City.
  - LU-G-3 Promote sustainability in the design and development of public and private development projects.
  - LU-G-10: Foster viable, pedestrian-oriented neighborhood centers with vertically- and horizontally- mixed-use development.
- b. Development of the site as proposed, including personal storage, requires approval of a Conditional Use Permit and would be subject to the City's development standards.

- c. An amendment to the General Plan designation is being processed concurrently with this Zone Change request. Approval of the Zone Change is contingent upon the approval of General Plan Amendment 2012-002 G, to ensure consistency between the General Plan and Zoning. The commercial mixed use (CMX) zoning will allow for similar types of land uses but in different proportions than currently exist on the property. In addition, the CMX Zone will allow the personal storage development pursuant to approval of a Conditional Use Permit as well as the drive through lanes as proposed.
- d. The subject Zone Change will not create adverse environmental impacts on the adjacent neighborhood when mitigation measures are implemented and standards of the Development Ordinance and General Plan are applied to the subsequent development project.

NOW, THEREFORE, BE IT ORDAINED: That the City Council of the City of Porterville does ordain as follows:

Section 1: That the following described property in the City of Porterville, County of Tulare, State of California, known as Zone Change 2012-002-Z, is hereby rezoned from RM-3 (High Density Residential), RM-2 (Medium Density Residential), and CN (Neighborhood Commercial) to CMX (Commercial Mixed Use), pursuant to Section 3 below, for the parcel described herein as Assessor's Parcel Number 245-410-032 located on the south side of Henderson Avenue, west of Newcomb Street; and

Section 2: It is further ordained that all records of the City of Porterville, together with the official zoning map of the City of Porterville, shall be changed to show the above described real property is rezoned from RM-3 (High Density Residential), RM-2 (Medium Density Residential), and CN (Neighborhood Commercial) to CMX (Commercial Mixed Use) for the site described above, more particularly shown on the attached map as Exhibit "A"; and

Section 3: This ordinance shall be in full force and effect not sooner than thirty (30) days from and after the ordinance's publication and passage, subject to the following conditions:

1. The project shall comply with all local, State, and federal laws.
2. The project shall comply with all mitigation measures identified in the approved CEQA document.
3. Building Design Criteria: In order to receive building permit approval for the mixed use project, City staff shall find that all of the following criteria have been met:
  - a. Integrated Theme – All buildings within the mixed use development will exhibit an integrated architectural theme that includes consistent materials, colors, and design details as represented on Exhibits "B, C and D"; including a complete master site plan, building elevations, and landscaping.



- b. The architectural details of building entrances shall be integrated with the overall building design in terms of materials, scale, proportion, and design elements.
  - c. All buildings shall include a complementary level of design detail on all facades.
  - d. Signage shall be in compliance with the Master Sign Program for the entire development, as represented herein on Exhibit "E".
4. The entire commercial mixed use project is considered one project as shown on the proposed Master Development Plan and Tentative Parcel Map (Exhibits "B" and "F"). Building permits secured separately for phased buildings and buildings with differing uses are considered part of one overall commercial mixed use project.

As either or both Parcels 2 and/or 3 develop, all parking, building pads, drive aisles, enhanced public open spaces, features and amenities shall be constructed with the initial building permit. The remaining building pad areas may develop by separate permits.

5. The main access point for the residential component (Parcel 4), along Henderson Avenue shall be fully developed and provide connectivity to the street with the first building permit for any portion of the apartments. The connecting drive aisle and parking within this area, as shown on Exhibit "B", (approximately 80 foot width), shall be developed prior to issuance of a certificate of occupancy. A recorded access easement for ingress/egress, parking, trash and connectivity to both streets shall serve as a guarantee for the functionality of the apartments on Parcel 4 and the adjacent development to the east. As shown on Exhibit "B", the section of the easement running east to west shall be developed in favor of Parcel 4 and the section of the easement running north to south shall be in favor of the adjacent parcel.
6. All improvements shall be in accordance with City standards and should maintain a common theme throughout the entire project. All landscaping shall comply with all City of Porterville landscape and irrigation standards. Plants shall be selected for their ability to prosper in the climate and geography of this region; the Sunset Garden manual is one such approved resource that is frequently used to guide plant selection. Such landscaping shall include an automatic sprinkler system and adequate maintenance to maintain the landscaping as approved free of diseased, dead or damaged materials. Plants shall be maintained in a healthy and vigorous growing condition and planting areas shall be maintained in a clean and orderly manner, free of weeds and debris. Other improvements to the Land Area are to include, at a minimum, a seating area with one or more benches and a trash receptacle which are designed for use in the public space. Owner shall submit details regarding the style, color, and materials for approval of the Community Development Director, which approval shall not be unreasonably withheld.

7. Development of the project area shall substantially comply with all plan sheets (Exhibits "B, C, and D").

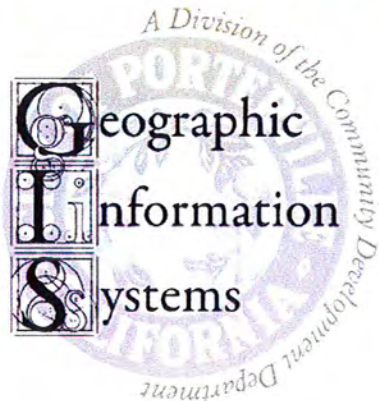
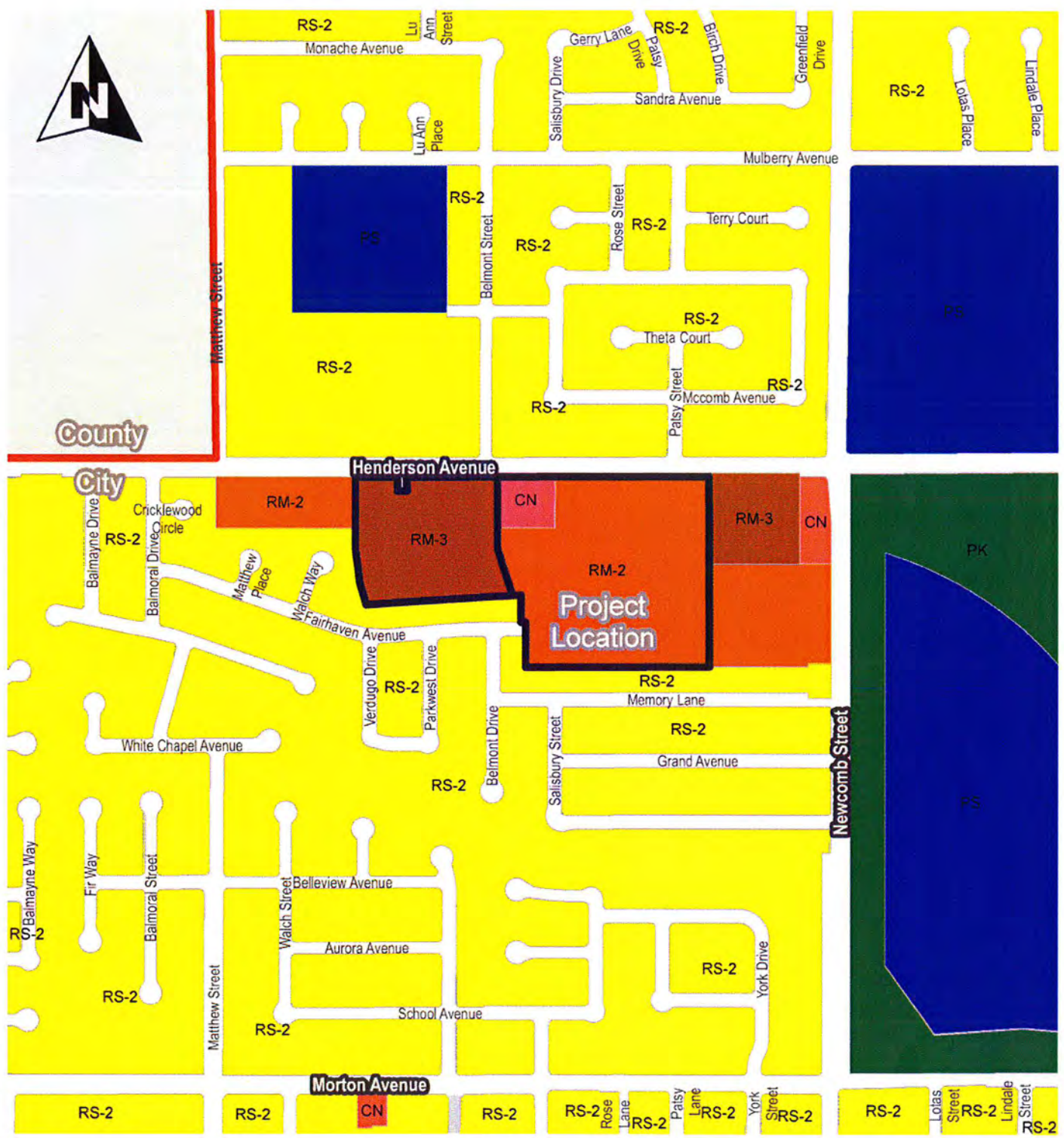
PASSED, APPROVED AND ADOPTED this \_\_\_\_\_ day of March, 2013.

By: \_\_\_\_\_  
Virginia R. Gurrola, Mayor

ATTEST:

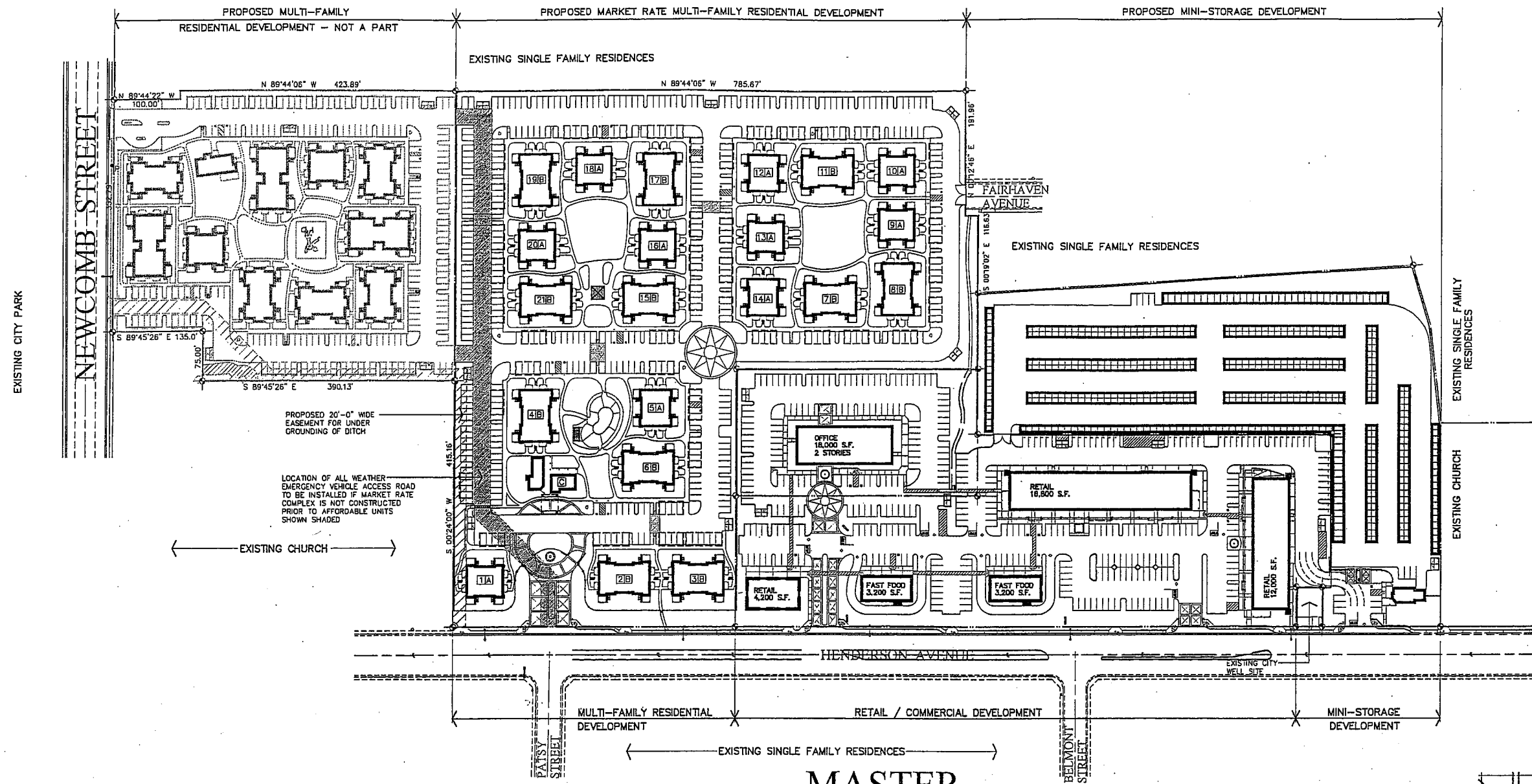
John D. Lollis, City Clerk

By: \_\_\_\_\_  
Patrice Hildreth, Chief Deputy City Clerk



PRC 2012-002  
Henderson Ave. Mixed Use  
Zoning Map  
1" = 600 ft.

EXHIBIT A



T. H. E.  
 VINCENT  
 COMPANY

ARCHITECT'S, INC.  
 1500 West Shaw, Ste. 30  
 Fresno, California 93711  
 Phone: 559.225.2602

Revision	Date

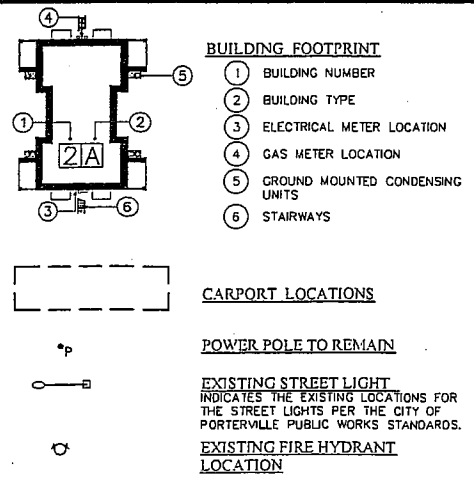
PROPOSED DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
 WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA

Project Architect:

# MASTER DEVELOPMENT PLAN



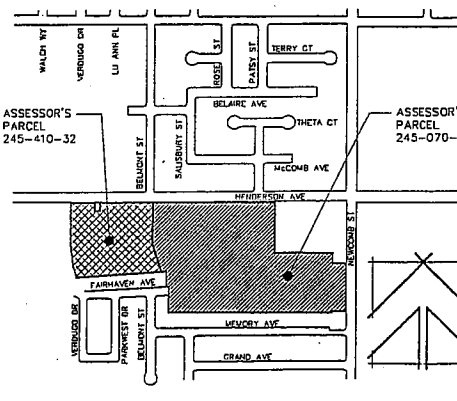
## SYMBOLS



## STATISTICS

PROJECT PROPOSAL	MULTI - FAMILY RESIDENTIAL		MASTER DEVELOPMENT	
SITE AREA	506,403 S.F. (11.6 ACRES)			
PROJECT COVERAGE				
a. BUILDING AREA (INCLUDES CARPORTS)	108,128 S.F. (21.4%)			
b. PARKING / PAVED AREA	153,456 S.F. (30.3%)			
c. LANDSCAPE AREA	244,819 S.F. (48.3%)			
PROPOSED PROJECT DENSITY	14.48 UNITS / ACRE			
REQUIRED PARKING				
a. RESIDENT PARKING				
1. COVERED PARKING	168 STALLS			
2. OPEN STALLS	128 STALLS			
b. GUEST PARKING	36 STALLS			
c. TOTAL PARKING REQUIRED	352 STALLS			
d. ACCESSIBLE STALLS				
1. COVERED STALLS @ 2%	4 STALLS			
2. OPEN STALLS @ 5%	9 STALLS			
PARKING PROVIDED				
a. CARPORTS	168 STALLS			
b. OPEN STALLS	268 STALLS			
c. TOTAL PARKING PROVIDED	436 STALLS			
d. ACCESSIBLE STALLS				
1. COVERED STALLS	4 STALLS			
2. OPEN STALLS	9 STALLS			

## VICINITY MAP



Issued:	Date:
DESIGN REVIEW	03.08.12
PLAN CHECK	
BACK CHECK	
PERMITS	
CONSTRUCTION	
AS BUILT	

All drawings and written material appearing herein constitute original work of the architect and may not be duplicated, used, or disclosed without written consent of the architect.

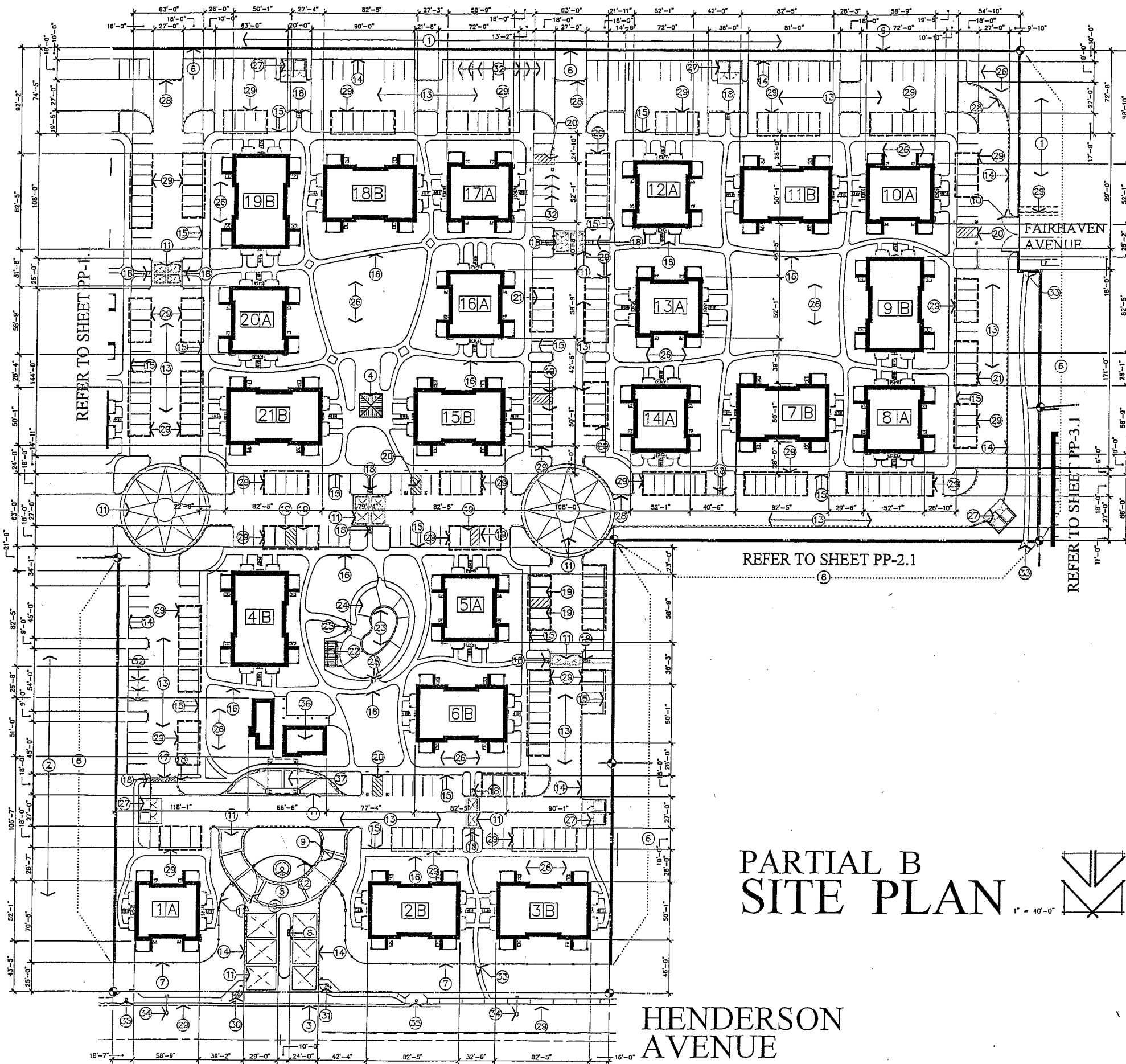
Reference North: [Symbol]

Scale: 1" = 80'-0"

Project Name: PACIFIC RIM  
 PORTERVILLE, CA  
 Project Number: 110926  
 Plot Date: 01.30.13  
 Sheet Number:

**PP1.1**  
 Of \_\_\_\_\_ Sheets

**EXHIBIT B-1**



# REFERENCE PLAN THE

**VINCENT COMPANY ARCHITECTS, INC.**  
 1500 West Shaw, Ste. 30  
 Fresno, California 93711  
 Phone: 559.225.2602

Revisions: \_\_\_\_\_ Date: \_\_\_\_\_

NOTE: SHADING INDICATES THE PORTION INCLUDED ON THIS PLAN

## KEYNOTES

- 1 EXISTING ADJACENT SINGLE FAMILY RESIDENTIAL DEVELOPMENT TO REMAIN.
- 2 EXISTING ADJACENT CHURCH TO REMAIN
- 3 EXISTING RIGHT-OF-WAY IMPROVEMENTS ALONG THE STREET FRONTAGE TO INCLUDE CONCRETE CURB, GUTTER, AND SIDEWALK PER CITY OF PORTERVILLE PUBLIC WORKS DEPARTMENT STANDARDS TO REMAIN.
- 4 INDICATES THE LOCATION OF COVERED BAR-B-QUE ARBOR AND CONCRETE HARDSCAPE.
- 5 FOUNTAIN LOCATION. COORDINATE THE WORK WITH THE LANDSCAPE PLAN/TYPICAL
- 6 INSTALL 6"-0" HIGH SOLID GROUTED REINFORCED CONCRETE MASONRY WALL AT PROPERTY LINE/ TYPICAL
- 7 INDICATES LOCATION OF 6"-0" HIGH ORNAMENTAL WROUGHT IRON FENCE WITH CRIMP TOP PICKETS AND MASONRY PLASTERS AT 24"-0" ON-CENTER (MAXIMUM) TO BE INSTALLED AT STREET FRONTAGE/ TYPICAL
- 8 INDICATES LOCATION OF PROJECT DIRECTION AND VISITOR CALL BOX INSTALLATION SHALL COMPLY WITH CITY OF PORTERVILLE PUBLIC WORKS AND FIRE DEPARTMENT REQUIREMENTS/ TYPICAL
- 9 INDICATES LOCATION OF 20"-0" WIDE x 6"-0" HIGH ORNAMENTAL WROUGHT IRON GATE(S) WITH AUTOMATIC OPERATORS. INSTALLATION SHALL COMPLY WITH CITY OF PORTERVILLE PUBLIC WORKS AND FIRE DEPARTMENT REQUIREMENTS/ TYPICAL
- 10 INDICATES LOCATION OF 20"-0" WIDE x 6"-0" HIGH CORRUGATED STEEL GATES FOR EMERGENCY VEHICLE INGRESS AND EGRESS ONLY. INSTALLATION SHALL COMPLY WITH CITY OF PORTERVILLE PUBLIC WORKS AND FIRE DEPARTMENT REQUIREMENTS/ TYPICAL
- 11 INDICATES LOCATION OF COLORED CONCRETE ACCENT PAVING WITH TILED JOINT PATTERN AS INDICATED. COORDINATE INSTALLATION WITH THE GRADING AND DRAINAGE PLAN/ TYPICAL
- 12 6"-0" HIGH "SIGNAGE" WALLS WITH CEMENT PLASTER FINISH INSTALLED OVER SOLID GROUTED REINFORCED CONCRETE MASONRY UNITS/ TYPICAL.
- 13 ASPHALTIC CONCRETE PAVING OVER CLASS II AGGREGATE BASE OVER NATIVE SOIL COMPACTED TO 90%. COORDINATE PAVING SECTIONS) WITH GRADING AND DRAINAGE PLAN AND PROJECT SOILS REPORT/ TYPICAL
- 14 6 INCH CONTINUOUS CONCRETE CURB/ TYPICAL
- 15 6 INCH CONTINUOUS CONCRETE CURB WITH INTEGRAL 4 INCH THICK CONCRETE WALK/ TYPICAL
- 16 4"-0" WIDE x 4 INCH THICK CONCRETE WALKS APPLIED OVER COMPACTED NATIVE SOIL (PROVIDE 7"-0" WIDE WALKS ADJACENT TO PARKING SPACES)/ TYPICAL
- 17 INSTALL 4'-0" WIDE PAINTED CROSSWALK AT DRIVE WHERE THE ACCESSIBLE PATH-OF-TRAVEL OCCURS. THE MAXIMUM SLOPE AT PAVING SHALL BE 5% WITH A 2% MAXIMUM CROSS SLOPE/ TYPICAL
- 18 INDICATES THE LOCATION OF DEPRESSED WALKWAY WHEEL CHAIR RAMP/ TYPICAL
- 19 INDICATES LOCATION OF ACCESSIBLE PARKING STALL WITH 5'-0" WIDE UNLOADING AREA AND WARPED WALK WHEELCHAIR RAMP. INSTALL SIGNAGE AND MARKING AS REQUIRED BY TITLE 24 CCR ACCESSIBILITY REQUIREMENTS/ TYPICAL
- 20 INDICATES LOCATION OF ACCESSIBLE PARKING STALL WITH 8'-0" WIDE VAN ACCESSIBLE UNLOADING AREA AND WARPED WALK WHEELCHAIR RAMP. INSTALL SIGNAGE AND MARKING AS REQUIRED BY TITLE 24 CCR ACCESSIBILITY REQUIREMENTS/ TYPICAL
- 21 INDICATES LOCATION OF ACCESSIBLE CARPORT STALL WITH 8'-0" WIDE VAN ACCESSIBLE UNLOADING AREA, 8'-2" MINIMUM VERTICAL CLEARANCE AND WARPED WALK WHEELCHAIR RAMP AVAILABLE FOR ASSIGNMENT TO TENANTS WITH PHYSICAL DISABILITIES/ TYPICAL
- 22 INDICATES LOCATION(S) OF WOOD ARBOR / SHADE STRUCTURE WITH REINFORCED CONCRETE MASONRY SUPPORT COLUMNS/ TYPICAL
- 23 INDICATES LOCATION OF SWIMMING POOL AND COOL DECKING TO BE INSTALLED UNDER SEPARATE PERMIT AND PLAN CHECK. POOL CONTRACTOR SHALL SUBMIT DRAWINGS AND CALCULATIONS TO THE CITY OF PORTERVILLE FOR PLAN CHECK AND PERMITS PRIOR TO COMMENCING WORK/ TYPICAL
- 24 COORDINATE INSTALLATION OF POOL DECKING WITH POOL CONTRACTOR. PROVIDE 1/4 INCH PER FOOT MAXIMUM CROSS SLOPE AT DECK/ TYPICAL
- 25 INDICATES THE LOCATION OF 3'-0" WIDE x 5'-0" HIGH WROUGHT IRON PEDESTRIAN GATE WITH PANIC HARDWARE (APPROVED FOR EXTERIOR INSTALLATION) WITH LEVER TYPE EXTERIOR HARDWARE (CONFORM KEYING REQUIREMENTS WITH THE OWNER).
- 26 LANDSCAPE AREAS. COORDINATE WORK WITH LANDSCAPE PLANS/ TYPICAL.
- 27 INDICATES LOCATION OF TRASH / RECYCLING ENCLOSURE WITH REINFORCED CONCRETE MASONRY WALLS TO 4'-8" PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL
- 28 DASHED LINE INDICATES LOCATION(S) OF CURBS TO BE PAINTED RED AND MARKED "FIRE LANE - NO PARKING". LETTERS SHALL BE WHITE, 3 INCHES HIGH AND SPACED AT 50'-0" ON-CENTER PER CITY OF PORTERVILLE FIRE DEPARTMENT STANDARDS/ TYPICAL
- 29 INDICATES THE LOCATION(S) OF PRE-FABRICATED CARPORTS TO BE INSTALLED UNDER SEPARATE PERMIT / TYPICAL
- 30 INSTALL 30" STATE STANDARD "STOP" SIGN(S) AT LOCATION(S) SHOWN. SIGN SHALL BE MOUNTED ON A 2" GALVANIZED POST WITH THE BOTTOM OF THE SIGN 7" ABOVE GROUND, LOCATED BEHIND CURB AND IMMEDIATELY BEHIND MAJOR STREET SIDEWALK. "RIGHT TURN ONLY" SIGN IS ALSO REQUIRED AT SAME LOCATION. INSTALL 30" x 36" STATE STANDARD SIGN IMMEDIATELY BELOW THE STOP SIGN ON SAME POST.
- 31 PROVIDE SIGN AT EACH SITE ENTRANCE THAT CLEARLY STATES, IF HIGH LETTERING STATING: "UNAUTHORIZED VEHICLES PARKED IN DESIGNATED ACCESSIBLE SPACES NOT DISPLAYING DISTINGUISHING PLACARDS OR SPECIAL LICENSE PLATES ISSUED FOR PERSONS WITH DISABILITIES WILL BE TOWED AWAY AT OWNER'S EXPENSE. TOWED VEHICLES MAY BE RECLAIMED BY CONTACTING THE CITY OF SAN JOSE POLICE DEPARTMENT.
- 32 4 INCH WIDE WHITE PAINTED STRIPING PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL AT PARKING STALLS.
- 33 INSTALL 3'-0" WIDE x 0'-0" HIGH WROUGHT IRON PEDESTRIAN GATE WITH LEVER TYPE LATCHSET.
- 34 INDICATES THE LOCATION(S) OF EXISTING STREET LIGHTS TO REMAIN
- 35 INDICATES THE LOCATION(S) OF EXISTING POWER POLES TO REMAIN
- 36 PROPOSED OFFICE / COMMUNITY BUILDING
- 37 PROPOSED PORTE COCHERE.

PROPOSED MULTI-FAMILY DEVELOPMENT FOR:

**PACIFIC RIM COMPANIES**

WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA

Project Architect:



Issuance:	Date:
<input checked="" type="checkbox"/> DESIGN REVIEW	03.03.12
<input type="checkbox"/> PLAN CHECK	
<input type="checkbox"/> BACK CHECK	
<input type="checkbox"/> PERMITS	
<input type="checkbox"/> CONSTRUCTION	
<input type="checkbox"/> AS BUILT	

All drawings and written material appearing herein constitute original work of the architect and may not be duplicated, used, or disclosed without written consent of the architect.

Reference North:

Scale: 1" = 40'-0"

Project Name: PACIFIC RIM  
 PORTERVILLE, CA.

Project Number: 110925

Plot Date: 09.26.12

Sheet Number:

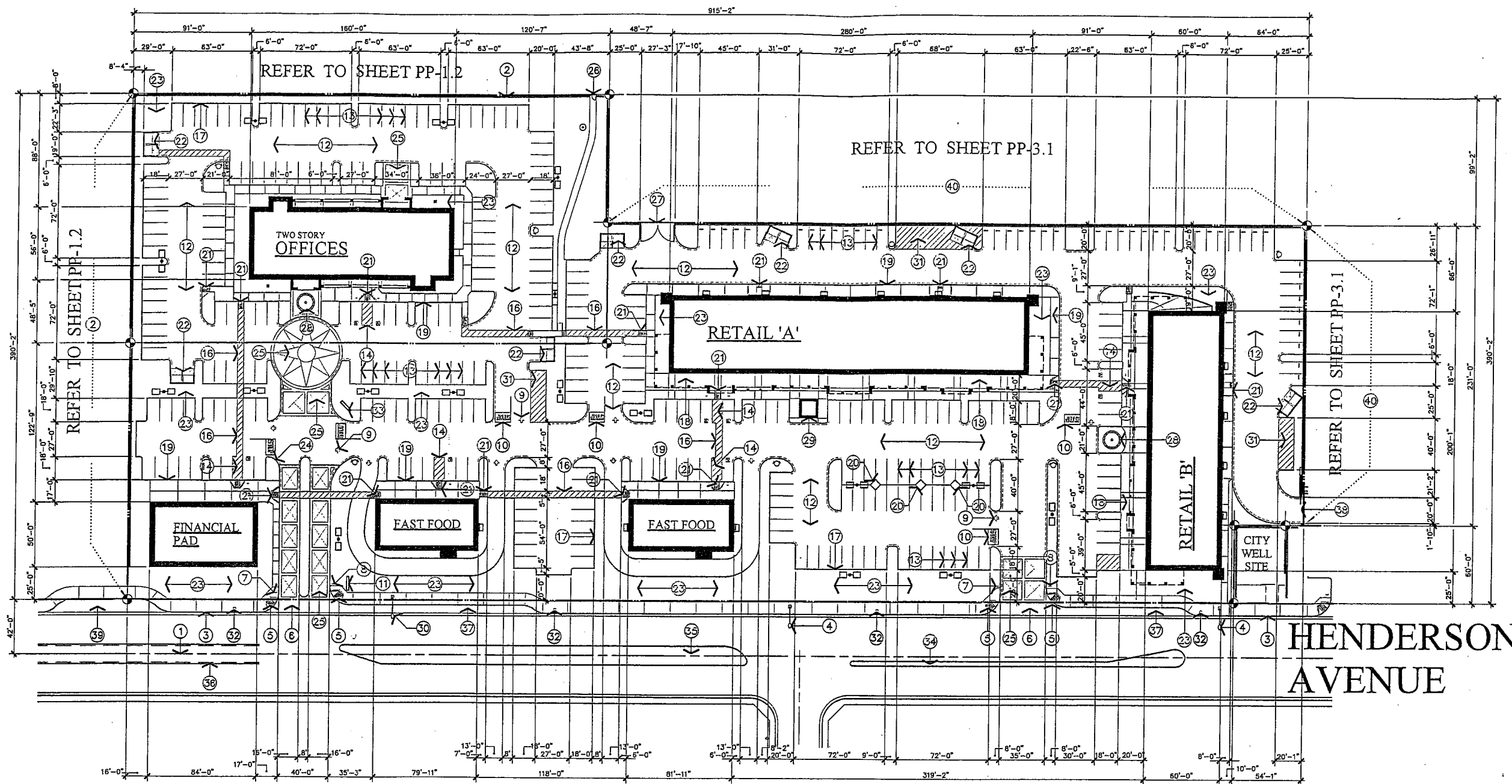
**PP1.2**

01 Sheets

# PARTIAL B SITE PLAN

HENDERSON AVENUE

EXHIBIT B-2

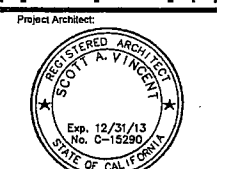


**THE VINCENT COMPANY**

ARCHITECT'S, INC.  
1500 West Shaw, Ste. 30  
Fresno, California 93711  
Phone: 559.225.2602

Revisions	Date

PROPOSED MULTIFAMILY DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA



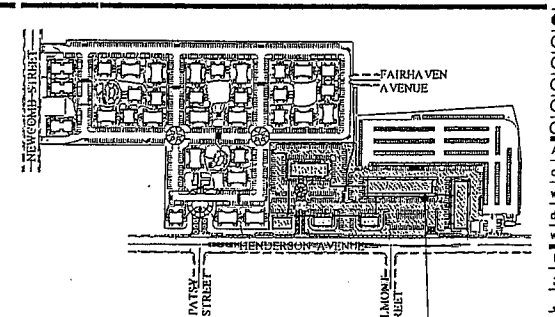
**STATISTICS**

PROPOSED USE(S)	NEIGHBORHOOD RETAIL SALE; FAST FOOD WITH DRIVE THROUGH; FINANCIAL; GENERAL OFFICE
<b>SITE AREA(S)</b>	
a. OFFICES	71,490 S.F. (1.64 ACRES)
b. COMMERCIAL/ RETAIL	227,816 S.F. (5.24 ACRES)
c. TOTAL SITE AREA	299,306 S.F. (6.88 ACRES)
<b>SITE COVERAGES</b>	
a. OFFICES	
1. BUILDING AREA	9,000 S.F. (12.6%)
2. PAVED AREA	43,180 S.F. (60.4%)
3. LANDSCAPE AREA	19,310 S.F. (27.0%)
b. COMMERCIAL/ RETAIL	
1. BUILDING AREA	39,400 S.F. (17.3%)
2. PAVED AREA	43,180 S.F. (54.8%)
3. LANDSCAPE AREA	63,554 S.F. (27.9%)
<b>BUILDING AREA CALCULATIONS</b>	
a. TWO STORY OFFICE	18,000 S.F.
b. COMMERCIAL/ RETAIL	
1. RETAIL 'A'	16,800 S.F.
2. RETAIL 'B'	12,000 S.F.
3. FINANCIAL PAD	4,200 S.F.
4. FAST FOOD PAD 1	3,200 S.F.
5. FAST FOOD PAD 2	3,200 S.F.
6. TOTAL COMMERCIAL/ RETAIL	39,400 S.F.
<b>PARKING REQUIRED</b>	
a. OFFICE @ 1/250	72 STALLS
b. COMMERCIAL/ RETAIL	
1. RETAIL @ 1/250	116 STALLS
2. FAST FOOD @ 1/100	64 STALLS
3. FINANCIAL PAD @ 1/250	17 STALLS
TOTAL STALLS REQUIRED	269 STALLS
c. ACCESSIBLE STALLS REQUIRED	7 STALLS
<b>PARKING PROVIDED</b>	
a. OFFICE	107 STALLS
b. COMMERCIAL/ RETAIL	245 STALLS
TOTAL STALLS PROVIDED	352 STALLS
c. ACCESSIBLE STALLS PROVIDED	9 STALLS
<b>LOADING ZONES</b>	3 ZONES

**KEYNOTES**

- 1 INDICATES SECTION LINE/ CENTERLINE OF THE STREET.
- 2 INSTALL 6'-0" HIGH SOLID GROUTED REINFORCED CONCRETE MASONRY WALL AT PROPERTY LINE/ TYPICAL.
- 3 EXISTING CONCRETE CURB, GUTTER, INSTALL 9'-6" WIDE CONCRETE WALK FOR ENTIRE STREET FRONTAGE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS TO REMAIN.
- 4 EXISTING STREET LIGHTS INSTALLED PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS TO REMAIN.
- 5 INSTALL ACCESSIBLE OFF-SITE CURB RAMP WITH TRUNCATED DOMES PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 6 INSTALL COMMERCIAL STREET-TYPE APPROACHES PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 7 INDICATES THE LOCATION OF 30" STATE STANDARD "STOP" SIGN(S). SIGN SHALL BE MOUNTED ON A 2 INCH DIAMETER GALVANIZED POST WITH THE BOTTOM OF THE SIGN 7'-0" ABOVE GROUND AND LOCATED BEHIND CURB AND IMMEDIATELY BEHIND MAJOR STREET SIDEWALK/ TYPICAL.
- 8 INDICATES THE LOCATION OF 17"x22" (MINIMUM) PARKING FOR PERSONS WITH DISABILITIES ONLY SIGNAGE AT OFF-STREET ENTRANCES TO PROPERTY.
- 9 INDICATES LOCATION OF NEW PAINTED TRAFFIC DIRECTIONAL ARROWS PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 10 INDICATES LOCATION OF NEW PAINTED TRAFFIC SIGNAGE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 11 INDICATES LOCATION OF PROPOSED POLE MOUNTED PROJECT SIGN (TO BE REVIEWED AND APPROVED UNDER SEPARATE PERMIT). COORDINATE INSTALLATION OF POWER SUPPLY TO THIS LOCATION WITH THE ELECTRICAL SITE PLAN/ TYPICAL.
- 12 ASPHALTIC CONCRETE PAVING OVER CLASS II AGGREGATE BASE OVER NATIVE SOIL COMPACTED TO 90%. COORDINATE PAVING SECTION(S) WITH GRADING AND DRAINAGE PLAN AND PROJECT SIGN REPORT/ TYPICAL.
- 13 INSTALL 4 INCH WIDE WHITE PAINTED STALL STRIPING PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 14 8'-0" WIDE DISABLED ACCESSIBLE UNLOADING SPACE WITH RAMP ACCESS TO WALKWAY/ TYPICAL.
- 15 5'-0" WIDE DISABLED ACCESSIBLE UNLOADING SPACE WITH WHEELCHAIR RAMP ACCESS TO WALKWAY/ TYPICAL.
- 16 INSTALL 4'-0" WIDE PAINTED CROSSWALK WITH 5% MAXIMUM SLOPE AND 2% MAXIMUM CROSS SLOPE/ TYPICAL.
- 17 INDICATES LOCATION OF 6 INCH CONTINUOUS CURB/ TYPICAL.
- 18 INDICATES NEW 4 INCH THICK CONCRETE WALK WITH NON-SLIP FINISH PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 19 INDICATES LOCATION OF 6 INCH CONTINUOUS CURB WITH INTEGRAL WALKWAY/ TYPICAL.
- 20 COORDINATE WORK AT TREE WELLS WITH THE LANDSCAPE PLAN/ TYPICAL.
- 21 INSTALL ACCESSIBLE CURB RAMP WITH TRUNCATED DOMES/ TYPICAL.
- 22 INDICATES LOCATION OF 6'-0" HIGH CONCRETE MASONRY TRASH ENCLOSURE WITH STUCCO FINISH TO MATCH THE ADJACENT BUILDING. PROVIDE (2) 6'-0" HIGH STEEL FRAME GATES WITH CORRUGATED DECKING AT OPENING/ TYPICAL.
- 23 LANDSCAPE AREA(S). COORDINATE THE WORK WITH THE LANDSCAPE PLAN/ TYPICAL.
- 24 DASHED LINE INDICATES LOCATION(S) OF CURBS TO BE PAINTED RED AND MARKED "FIRE LANE - NO PARKING". LETTERS SHALL BE WHITE, 3 INCHES HIGH AND SPACED AT 50'-0" ON-CENTER PER CITY OF PORTERVILLE FIRE DEPARTMENT STANDARDS/ TYPICAL.
- 25 INDICATES LOCATION OF COLORED CONCRETE ACCENT PAVING WITH TOOLED JOINT PATTERN AS INDICATED. COORDINATE INSTALLATION WITH THE GRADING AND DRAINAGE PLAN/ TYPICAL.
- 26 INSTALL 3'-0" WIDE x 6'-0" HIGH WROUGHT IRON PEDESTRIAN ACCESS GATE WITH LEVER TYPE LATCHSET.
- 27 INDICATES LOCATION OF 20'-0" WIDE x 6'-0" HIGH CORRUGATED STEEL EMERGENCY VEHICLE ACCESS ONLY GATE(S). INSTALLATION SHALL COMPLY WITH CITY OF PORTERVILLE PUBLIC WORKS AND FIRE DEPARTMENT REQUIREMENTS/ TYPICAL.
- 28 FOUNTAIN LOCATION. COORDINATE THE WORK WITH THE LANDSCAPE PLAN/ TYPICAL.
- 29 COORDINATE THE INSTALLATION OF THE FREE-STANDING CLOCK TOWER WITH THE EXTERIOR ELEVATIONS.
- 30 EXISTING STREET LIGHT TO BE RELOCATED/ TYPICAL.
- 31 INDICATES THE LOCATION OF COMMERCIAL LOADING ZONE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS.
- 32 INDICATES THE LOCATION OF EXISTING POWER POLE TO REMAIN.
- 33 INDICATES THE LOCATION OF ILLUMINATED PROJECT DIRECTORY TO BE INSTALLED UNDER SEPARATE SIGN PERMIT.
- 34 INSTALL MEDIAN ISLAND WITH LEFT-TURN POCKET PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS.
- 35 INSTALL MEDIAN ISLAND PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS.
- 36 INSTALL PAINTED MEDIAN FOR LEFT-TURN ACCESS PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS.
- 37 INSTALL RIGHT-TURN LANE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS. PROVIDE DEDICATION TO THE CITY AS REQUIRED FOR THE INSTALLATION.
- 38 INDICATES THE LOCATION OF 20'-0" WIDE x 6'-0" HIGH CORRUGATED STEEL "EXIT ONLY" GATE WITH AUTOMATIC OPERATION AND EXIT LOOP ONLY.
- 39 INSTALL BUS TURN-OUT PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS. PROVIDE DEDICATION TO THE CITY AS REQUIRED FOR THE INSTALLATION.
- 40 COORDINATE THE INSTALLATION OF REINFORCED CONCRETE MASONRY FENCES AT THE PROPERTY LINE WITH THE MINI STORAGE SITE PLAN, SHEET PP-3.1

**REFERENCE PLAN**



NOTE: SHADING INDICATES THE PORTION INCLUDED ON THIS PLAN

Issue/Date	Date
DESIGN REVIEW	03.08.12
PLAN CHECK	
BACK CHECK	
PERMITS	
CONSTRUCTION	
AS BUILT	

All drawings and written material appearing herein constitute original work of the architect and may not be duplicated, used, or disclosed without written consent of the architect.

Reference North:

Scale: 1" = 40'-0"

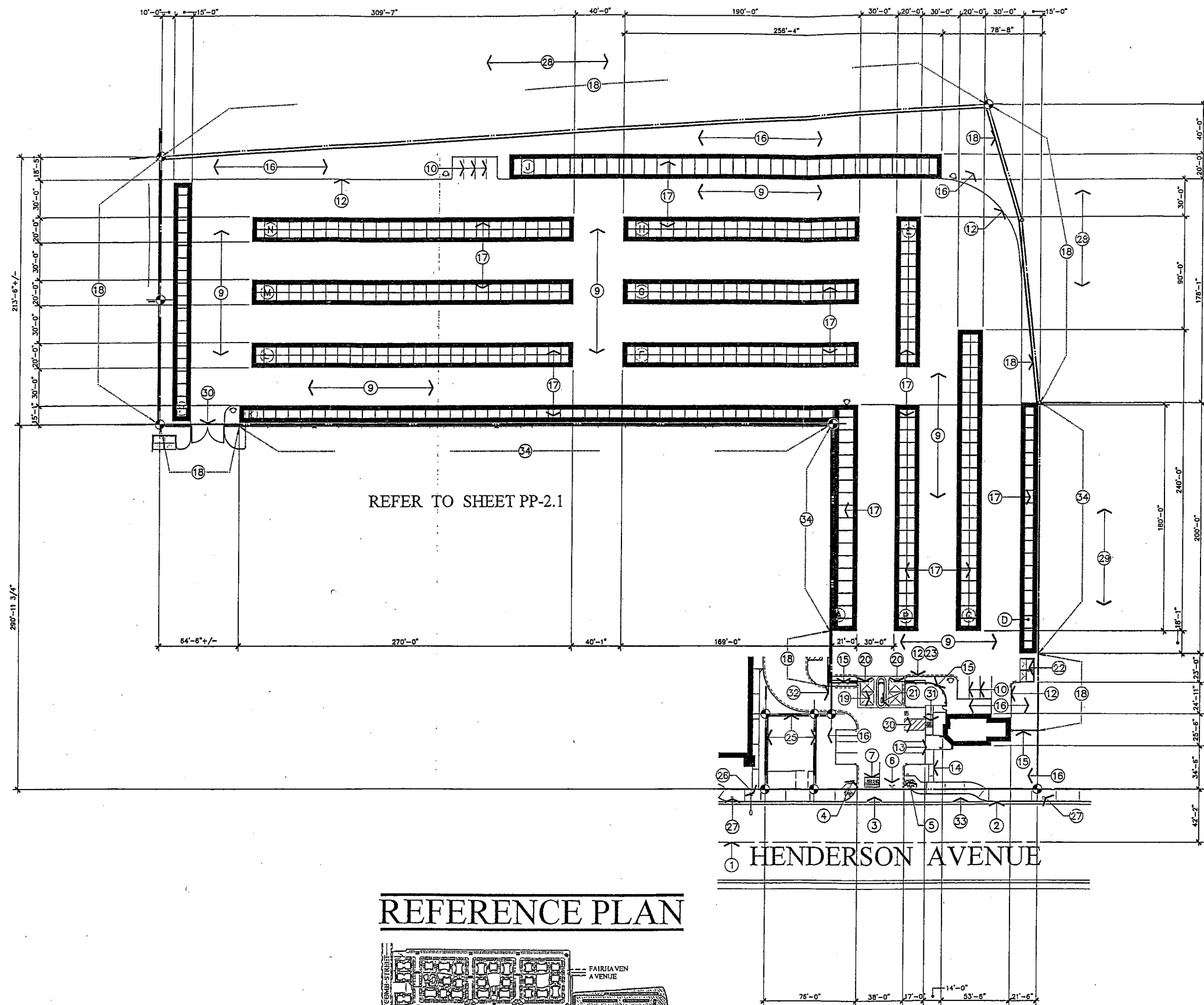
Project Name: PACIFIC RIM  
PORTERVILLE, CA

Project Number: 110925  
Plot Date: 09.11.12  
Sheet Number: PP2.1

**RETAIL CENTER SITE PLAN**

1" = 40'-0"

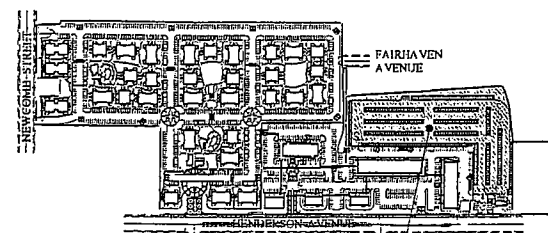
**EXHIBIT B-3**



REFER TO SHEET PP-2.1

HENDERSON AVENUE

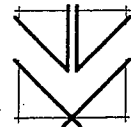
### REFERENCE PLAN



NOTE: SHADING INDICATES THE PORTION INCLUDED ON THIS PLAN

## MINI-STORAGE SITE PLAN

1" = 40'-0"



## STATISTICS

PROPOSED USE	NEIGHBORHOOD MINI STORAGE		
PROJECT ZONING	CMX		
SITE AREA	212,524 S.F. (4.88 ACRES)		
SITE COVERAGES			
a. BUILDING AREA	1,290 S.F. (0.6%)		
b. PAVED AREA	119,738 S.F. (56.3%)		
c. LANDSCAPE AREA	92,786 S.F. (43.7%)		
BUILDING AREA(S)			
1. STORAGE BUILDING			
BUILDING	USE/UNIT SIZE	NO. OF UNITS	BUILDING AREA
A	10 x 20	18	3,600 S.F.
B	10 x 10	36	3,600 S.F.
C	10 x 10	48	4,800 S.F.
D	10 x 15	20	3,000 S.F.
E	10 x 10	24	2,400 S.F.
F	10 x 10	30	3,000 S.F.
G	10 x 10	38	3,800 S.F.
H	10 x 10	38	3,800 S.F.
J	10 x 20	35	7,000 S.F.
K	10 x 15	48	7,200 S.F.
L	10 x 10	52	5,200 S.F.
M	10 x 10	52	5,200 S.F.
N	10 x 10	52	5,200 S.F.
P	10 x 15	19	2,850 S.F.
TOTAL UNITS/AREA		518 UNITS	61,450 S.F.
OFFICE BUILDING AREA			1,290 SQ. FT.
PARKING REQUIRED			
a.	1 STALL / 75 UNITS		7 STALLS
b.	1 STALL / 300 S.F. OFFICE		4 STALLS
c.	TOTAL STALLS REQUIRED		11 STALLS
d.	ACCESSIBLE STALLS		1 STALL
PARKING PROVIDED			
a.	OPEN STALLS		14 STALLS
b.	ACCESSIBLE STALLS		1 STALL

## KEYNOTES

- 1 INDICATES SECTION LINE/ CENTERLINE OF THE STREET.
- 2 EXISTING RIGHT-OF-WAY IMPROVEMENTS ALONG THE STREET FRONTAGE INCLUDING CONCRETE CURB, GUTTER, AND SIDEWALK PER CITY OF PORTERVILLE PUBLIC WORKS DEPARTMENT STANDARDS TO REMAIN.
- 3 INSTALL COMMERCIAL STREET-TYPE APPROACHES PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 4 INSTALL 30" STATE STANDARD "STOP" SIGN(S) AT LOCATIONS SHOWN. SIGN SHALL BE MOUNTED ON A 2 INCH DIAMETER GALVANIZED POST WITH THE BOTTOM OF THE SIGN 7'-0" ABOVE GROUND AND LOCATED BEHIND CURB AND IMMEDIATELY BEHIND MAJOR STREET SIDEWALK. "RIGHT TURN ONLY" SIGN AT SAME LOCATION. INSTALL 30"x36" STATE STANDARD SIGN IMMEDIATELY BELOW THE STOP SIGN ON THE SAME POST/ TYPICAL.
- 5 PROVIDE SIGN AT EACH SITE ENTRANCE THAT CLEARLY STATES, "HIGH LETTERING STATING: "UNAUTHORIZED VEHICLES PARKED IN DESIGNATED ACCESSIBLE SPACES NOT DISPLAYING Distinguishing PLACARDS OR SPECIAL LICENSE PLATES ISSUED FOR PERSONS WITH DISABILITIES WILL BE TOWED AWAY AT OWNER'S EXPENSE. TOWED VEHICLES MAY BE RECLAIMED BY CONTACTING THE CITY OF SAN JOSE POLICE DEPARTMENT."
- 6 INDICATES LOCATION OF NEW PAINTED TRAFFIC DIRECTIONAL ARROWS PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 7 INDICATES LOCATION OF NEW PAINTED TRAFFIC SIGNAGE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 8 INDICATES LOCATION OF PROPOSED POLE MOUNTED PROJECT SIGN (TO BE REVIEWED AND APPROVED UNDER SEPARATE PERMIT). COORDINATE INSTALLATION OF POWER SUPPLY TO THIS LOCATION WITH THE ELECTRICAL SITE PLAN/ TYPICAL.
- 9 INSTALL NEW CAST-IN-PLACE CONCRETE PAVING OVER COMPACTED NATIVE SOIL. COORDINATE THE INSTALLATION REQUIREMENTS WITH THE PAVING PLAN/ TYPICAL.
- 10 INSTALL 4 INCH WIDE WHITE PAINTED STALL STRIPING PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 11 INDICATES LOCATION OF ACCESSIBLE PARKING STALL WITH 8'-0" WIDE UNLOADING AREA AND WARRPED WALK WHEELCHAIR RAMP. INSTALL SIGNAGE AND MARKING AS REQUIRED BY TITLE 24 CCR ACCESSIBILITY REQUIREMENTS/ TYPICAL.
- 12 INDICATES LOCATION OF 6 INCH CONTINUOUS CURB/ TYPICAL.
- 13 6 INCH CONTINUOUS CONCRETE CURB WITH INTEGRAL 4 INCH THICK CONCRETE WALK/ TYPICAL.
- 14 INDICATES NEW 4 INCH THICK CONCRETE WALK WITH NON-SLIP FINISH PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 15 INDICATES LOCATION OF 6'-0" HIGH REINFORCED CONCRETE MASONRY FENCE (GROUT SOLID) WITH CEMENT PLASTER FINISH AND PILASTERS AT 20'-0" ON CENTER (INCREASE HEIGHT TO 8'-0" AT STORAGE BUILDINGS)/ TYPICAL.
- 16 LANDSCAPE AREA(S). COORDINATE THE WORK WITH THE CONCEPT LANDSCAPE PLAN/ TYPICAL.
- 17 STORAGE UNITS SHALL BE PRE-FABRICATED STEEL STORAGE UNITS TO BE INSTALLED UNDER SEPARATE BUILDING PERMITS/ TYPICAL.
- 18 INSTALL 6'-0" HIGH SOLID GROUTED REINFORCED CONCRETE MASONRY FENCE AT PROPERTY LINE/ TYPICAL.
- 19 INDICATES LOCATION OF COLORED CONCRETE ACCENT PAVING WITH TILED JOINT PATTERN AS INDICATED. COORDINATE INSTALLATION WITH THE GRADING AND DRAINAGE PLAN/ TYPICAL.
- 20 INDICATES THE LOCATION OF 6'-0" HIGH STEEL GATES WITH AUTOMATIC OPERATOR(S)/ TYPICAL.
- 21 INDICATES THE LOCATION OF PEDESTAL MOUNTED GATE CONTROLLER/ TYPICAL.
- 22 INDICATES LOCATION OF TRASH AND RECYCLING ENCLOSURE WITH REINFORCED CONCRETE MASONRY WALLS TO +4'-8" PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 23 DASHED LINE INDICATES LOCATION(S) OF CURBS TO BE PAINTED RED AND MARKED "FIRE LANE - NO PARKING". LETTERS SHALL BE WHITE, 3 INCHES HIGH AND SPACED AT 50'-0" ON-CENTER PER CITY OF PORTERVILLE FIRE DEPARTMENT STANDARDS/ TYPICAL.
- 24 INDICATES LOCATION OF 20'-0" WIDE x 6'-0" HIGH ORNAMENTAL WROUGHT IRON GATE(S) FOR EMERGENCY VEHICLE INGRESS AND EGRESS ON-Y. INSTALLATION SHALL COMPLY WITH CITY OF PORTERVILLE PUBLIC WORKS AND FIRE DEPARTMENT REQUIREMENTS/ TYPICAL.
- 25 EXISTING CONCRETE MASONRY WALL AT CITY OF PORTERVILLE WATER WELL SITE TO REMAIN.
- 26 EXISTING STREET LIGHTS INSTALLED PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS TO REMAIN.
- 27 INDICATES THE LOCATION OF EXISTING POWER POLE TO REMAIN.
- 28 EXISTING ADJACENT SINGLE FAMILY RESIDENTIAL DEVELOPMENT TO REMAIN.
- 29 EXISTING ADJACENT CHURCH TO REMAIN.
- 30 8'-0" WIDE DISABLED ACCESSIBLE UNLOADING SPACE WITH RAMP ACCESS TO WALKWAY/ TYPICAL.
- 31 INSTALL ACCESSIBLE CURB RAMP WITH TRUNCATED DOMES/ TYPICAL.
- 32 INDICATES THE LOCATION OF 20'-0" WIDE x 6'-0" HIGH CORRUGATED STEEL "EXIT ONLY" GATE (OMATIC OPERATION AND EXIT LOOP ONLY).
- 33 INSTALL RIGHT-TURN LANE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS. PROVIDE DEDICATION TO THE CITY AS REQUIRED FOR THE INSTALLATION.
- 34 INSTALL 9'-4" HIGH SOLID GROUTED REINFORCED CONCRETE MASONRY FENCE AT THE REAR WALLS OF STORAGE UNITS LOCATED AT THE PROPERTY LINE/ TYPICAL.

## T H E VINCENT COMPANY

ARCHITECT'S, INC.  
1500 West Shaw, Ste. 30  
Fresno, California 93711

Phone: 559.225.2602

Revisions	Date
△	
△	
△	
△	
△	
△	

PROPOSED MULTI-FAMILY DEVELOPMENT FOR:

**PACIFIC RIM DEVELOPMENT COMPANY**

WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA

Project Architect:

Issuance:	Date:
● DESIGN REVIEW	03.08.12
○ PLAN CHECK	
○ BACK CHECK	
○ PERMITS	
○ CONSTRUCTION	
○ AS BUILT	

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Reference North:

Scale: 1" = 40.00'

Project Name: PACIFIC RIM  
PORTERVILLE, CA

Project Number: 110925

Plot Date: 09.26.12

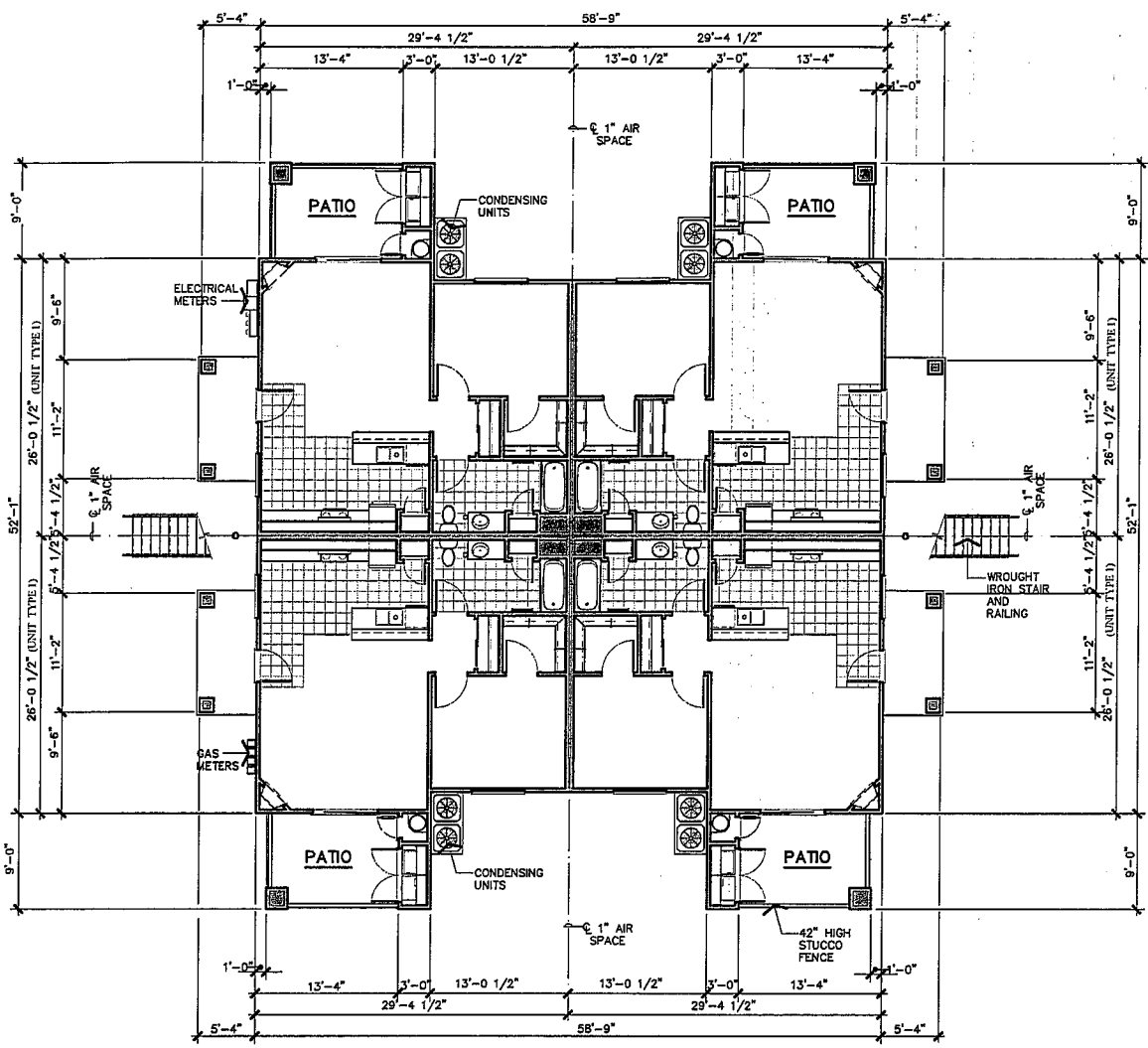
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**PP3.1**

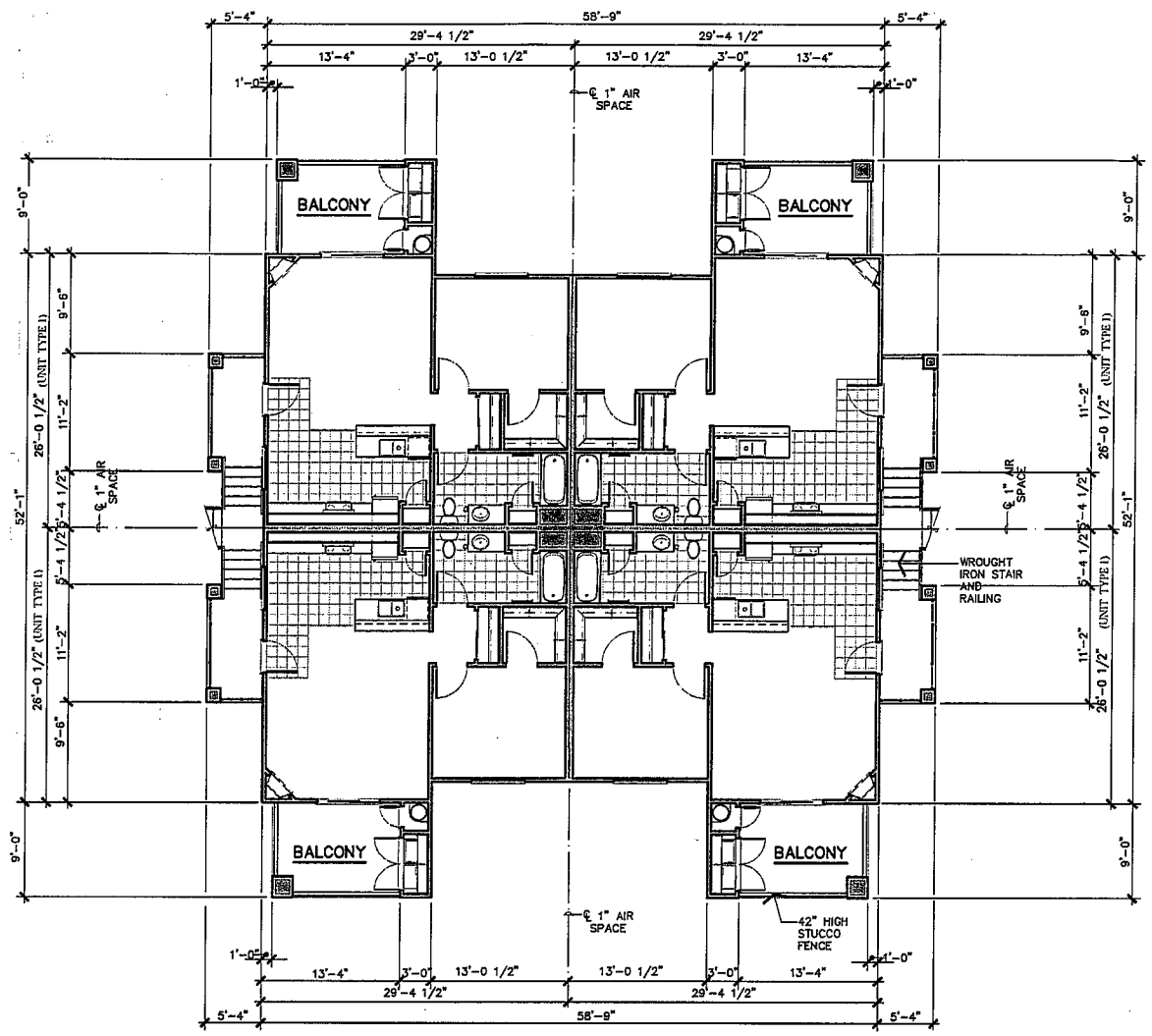
Of \_\_\_\_\_ Sheets

EXHIBIT B-4

Revisions	Date



**GROUND FLOOR**



**SECOND FLOOR**

PROPOSED MULTIFAMILY DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
 WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA

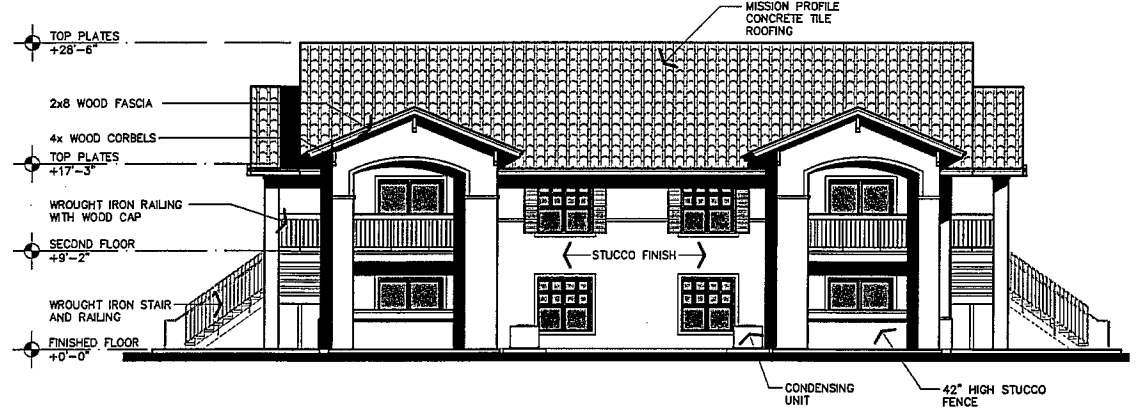
**BUILDING STATISTICS**

<b>BUILDING UNIT MIX</b>	(B) UNITS TYPE "1" 1 BEDROOM 1 BATHROOM WALK-UP UNIT 766 SQ. FT./UNIT
<b>OCCUPANCY GROUP</b>	R-2
<b>CONSTRUCTION TYPE</b>	V-B WITH NFPA 13R AUTOMATIC FIRE SPRINKLER SYSTEM
<b>BUILDING AREA</b>	
a. GROUND FLOOR	3,064 SQ. FT.
1. RESIDENTIAL UNITS	682 SQ. FT.
2. COVERED PORCH/ PATIOS	682 SQ. FT.
b. SECOND FLOOR	3,064 SQ. FT.
1. RESIDENTIAL UNITS	682 SQ. FT.
2. COVERED PORCH/ PATIOS	682 SQ. FT.
<b>TOTAL BUILDING AREA</b>	7,492 SQ. FT.
<b>FIRE RESISTANCE REQUIREMENTS</b>	
a. EXTERIOR WALLS	NON-RATED
b. INTERIOR BEARING WALLS	NON-RATED
c. INTERIOR NON-BEARING WALLS	NON-RATED
d. CEILING/ROOF ASSEMBLY	NON-RATED
e. FLOOR/CEILING ASSEMBLY	WJ-1.6 ONE HOUR RATED FLOOR/CEILING PER ASTM E 119/NFPA 251
f. UNIT COMMON WALLS	ONE HOUR RATED (G.A. FILE NO. WP-3370)
g. SHAFT ENCLOSURES	NONE OCCUR
h. EXTERIOR OPENINGS	NON-RATED

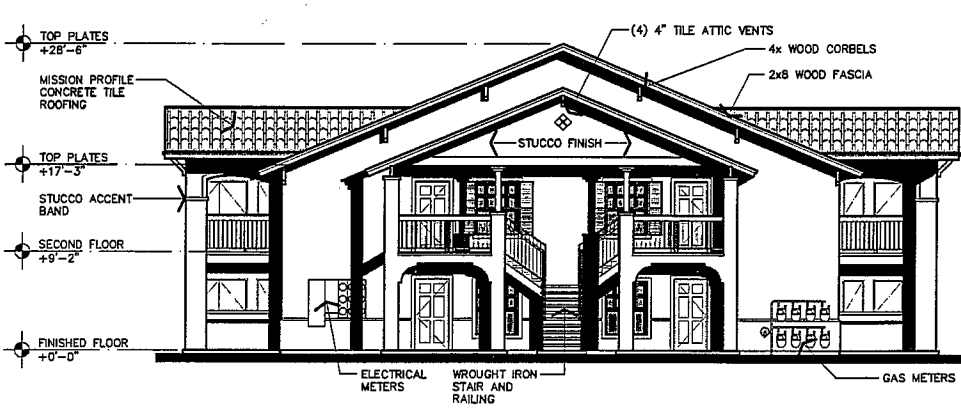


Issues:	Date:
● DESIGN REVIEW	03.08.12
○ PLAN CHECK	
○ BACK CHECK	
○ PERMITS	
○ CONSTRUCTION	
○ AS BUILT	

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**FRONT ELEVATION**



**SIDE ELEVATION**

**BUILDING A** 1/8" = 1'-0"

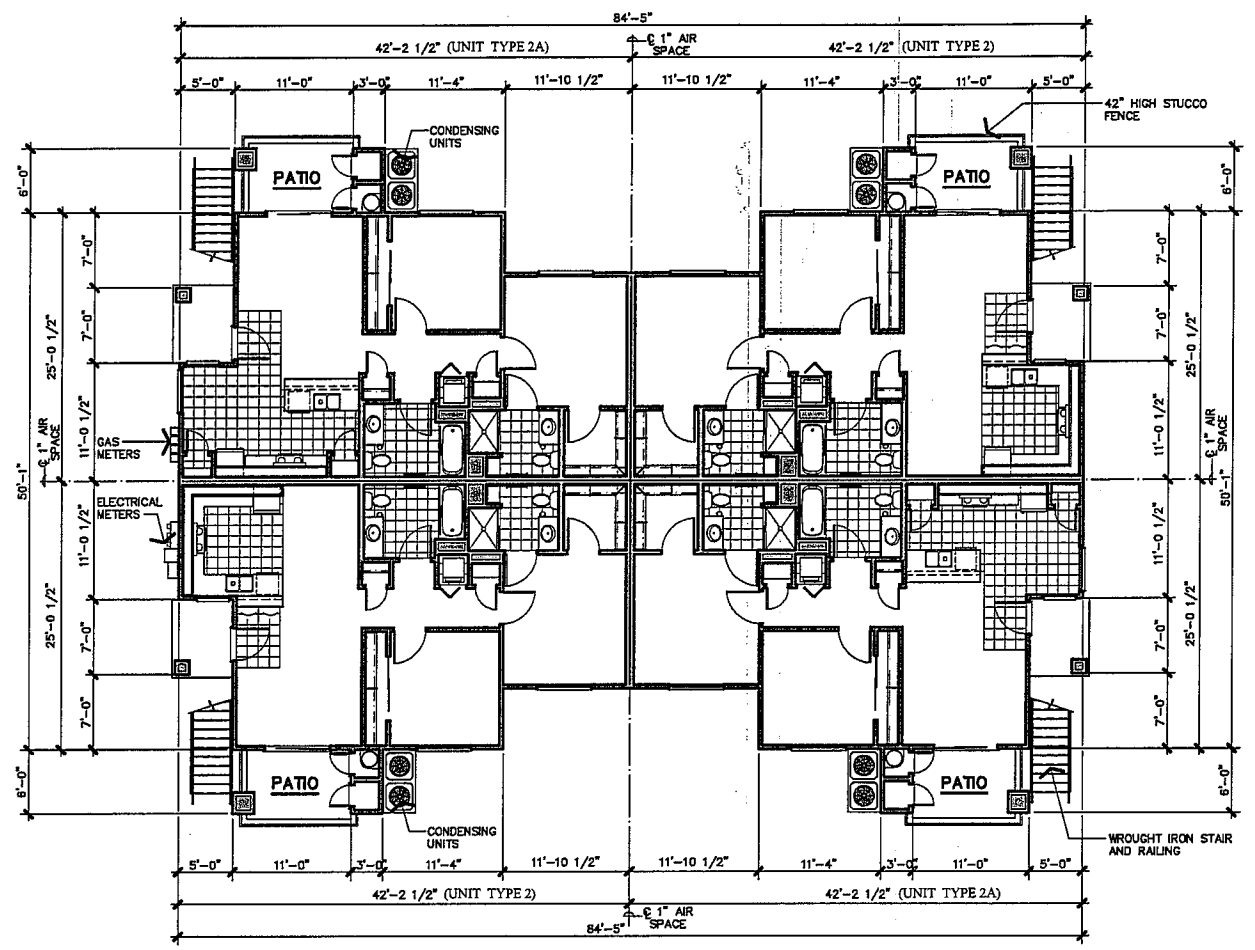
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Of . . . Sheets . . .

**EXHIBIT C-1**

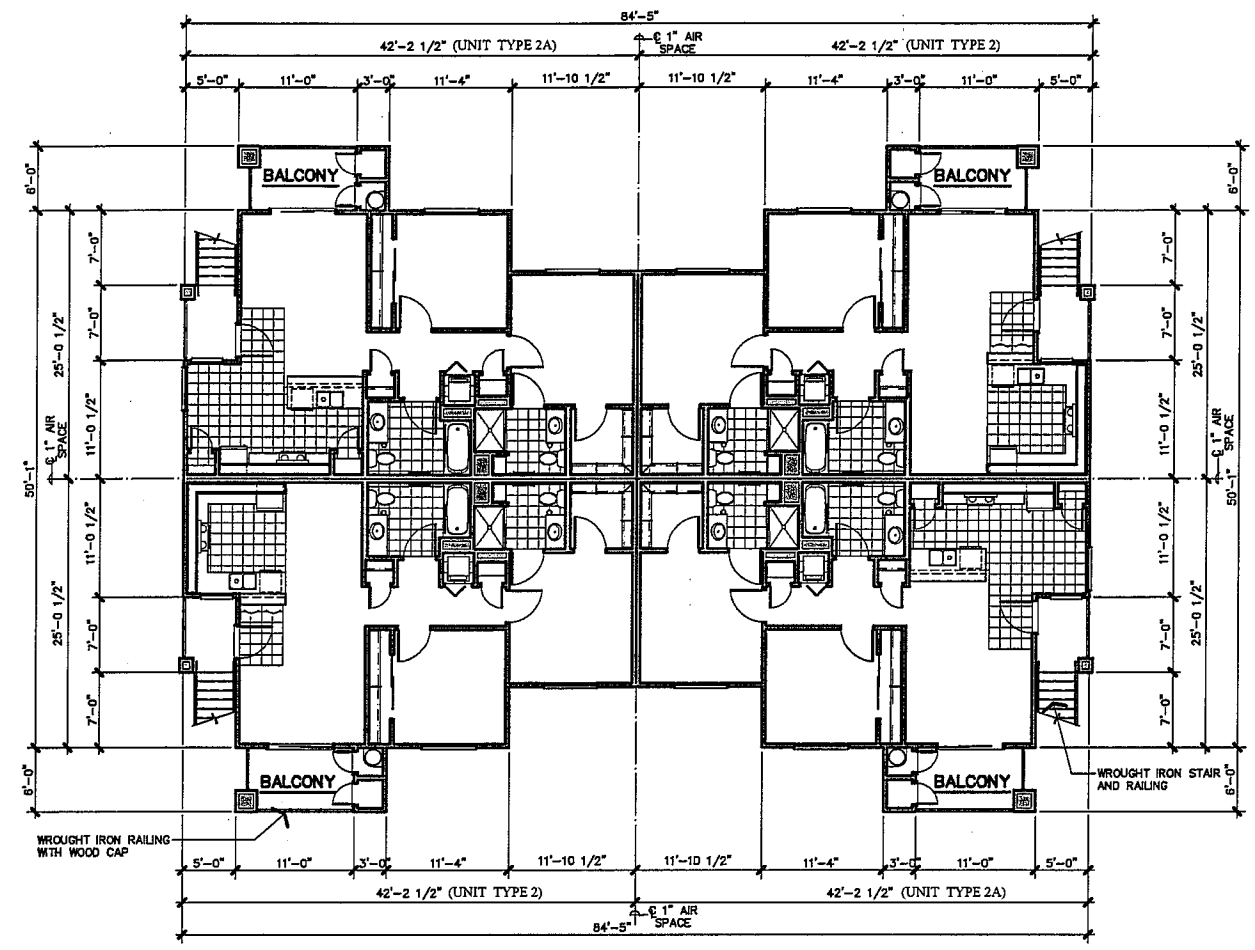
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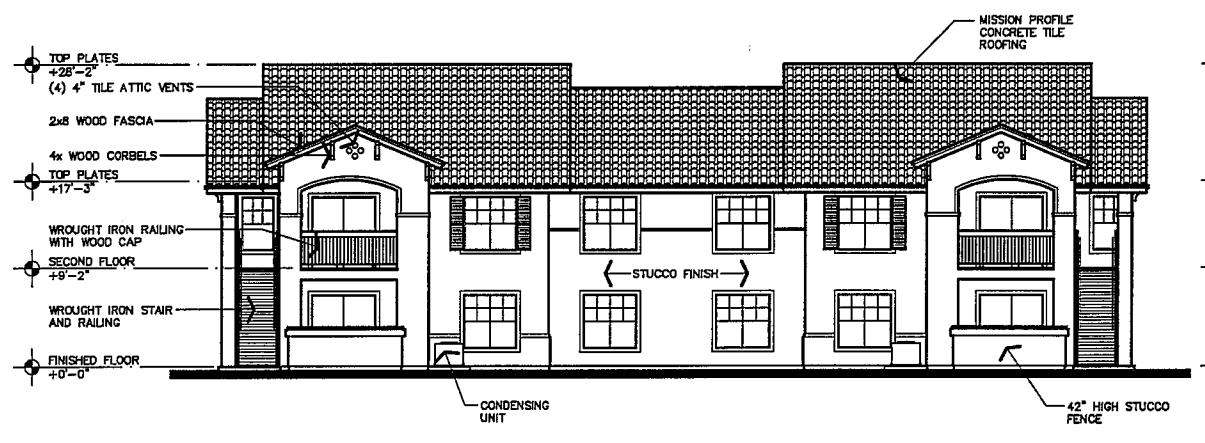
Revisions	Date
▲	
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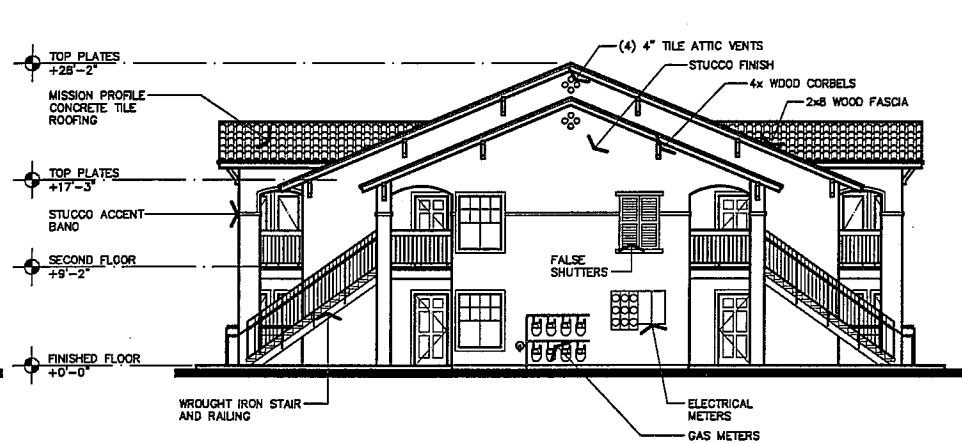
**GROUND FLOOR**



**SECOND FLOOR**



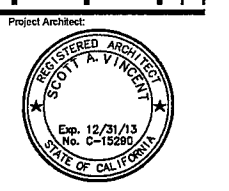
**FRONT ELEVATION**



**SIDE ELEVATION**

**BUILDING STATISTICS**

<b>BUILDING UNIT MIX</b>	(8) UNITS TYPE "2" 2 BEDROOMS 2 BATHROOMS WALK-UP UNIT 918 SQ. FT./UNIT
<b>OCCUPANCY GROUP</b>	R-2
<b>CONSTRUCTION TYPE</b>	V-B WITH NFPA 13R AUTOMATIC FIRE SPRINKLER SYSTEM
<b>BUILDING AREA</b>	
a. GROUND FLOOR	3,572 SQ. FT.
1. RESIDENTIAL UNITS	484 SQ. FT.
2. COVERED PORCH/PATIOS	
b. SECOND FLOOR	3,572 SQ. FT.
1. RESIDENTIAL UNITS	484 SQ. FT.
2. COVERED PORCH/PATIOS	
<b>TOTAL BUILDING AREA</b>	8,312 SQ. FT.
<b>FIRE RESISTANCE REQUIREMENTS</b>	
a. EXTERIOR WALLS	NON-RATED
b. INTERIOR BEARING WALLS	NON-RATED
c. INTERIOR NON-BEARING WALLS	NON-RATED
d. CEILING/ROOF ASSEMBLY	NON-RATED
e. FLOOR/CEILING ASSEMBLY	WN-1.6 ONE HOUR RATED FLOOR/CEILING PER ASTM E 119/NFPA 251
f. UNIT COMMON WALLS	ONE HOUR RATED (G.A. FILE NO. WP-337D)
g. SHAFT ENCLOSURES	NONE OCCUR
h. EXTERIOR OPENINGS	NON-RATED



Issues:	Date:
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<input type="checkbox"/> PLAN CHECK	
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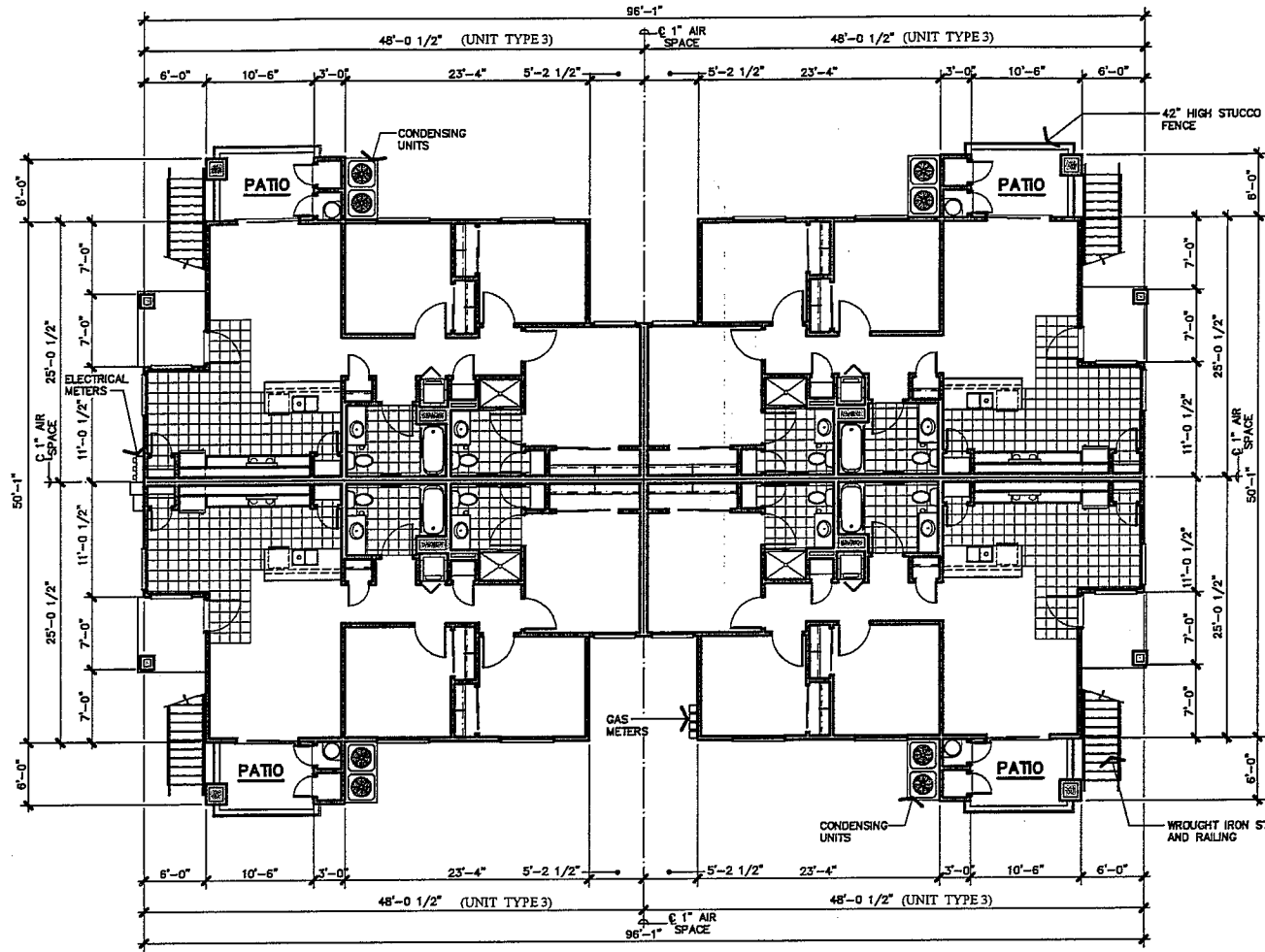
Reference North:  
Scale: 1/8" = 1'-0"  
Project Name: PACIFIC RIM  
PORTERVILLE, CA.  
Project Number: 110925  
Plot Date: 03.07.12  
Sheet Number:

**BUILDING B**

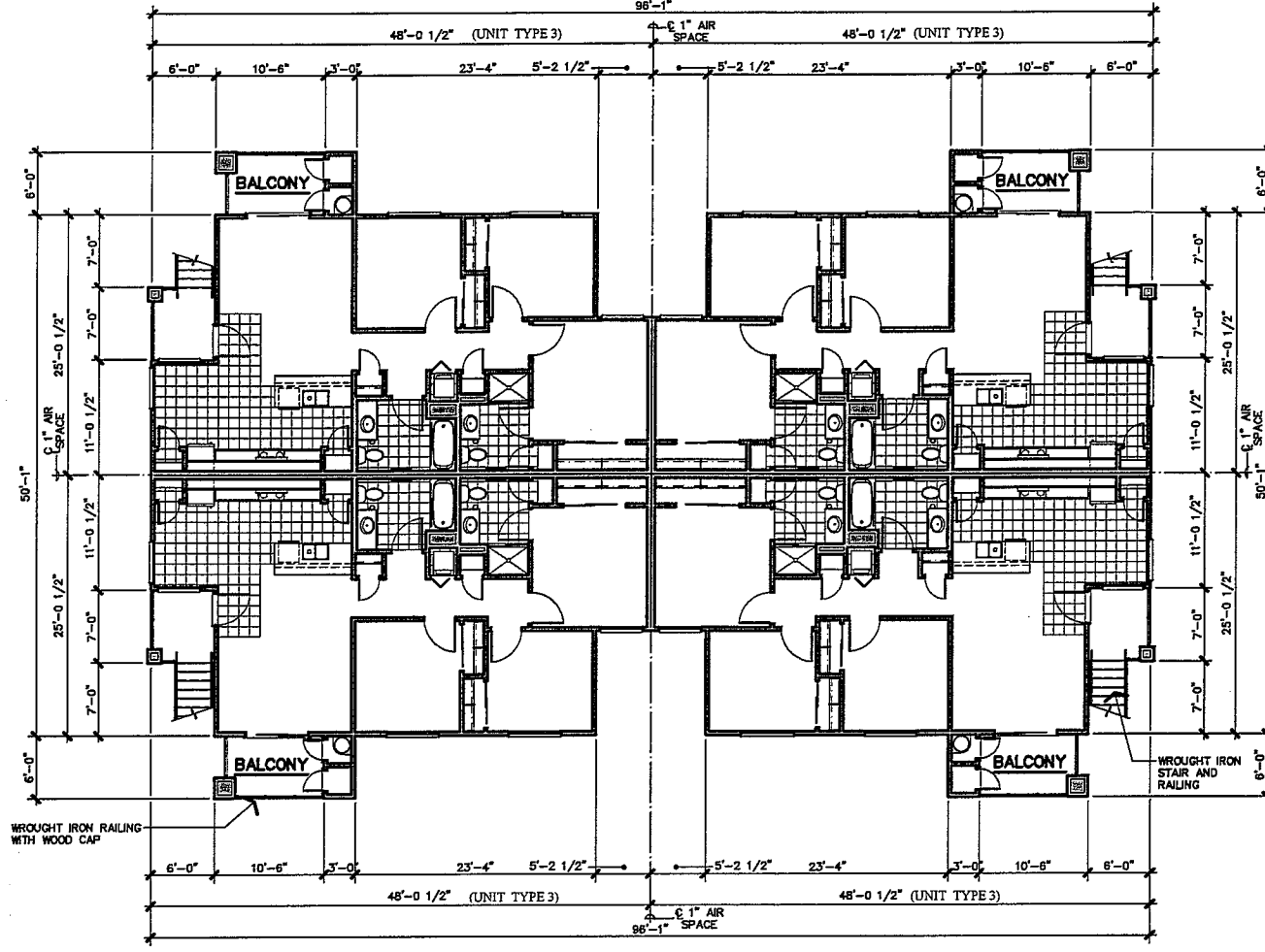
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1/8" = 1'-0"  
Of . . . Sheets . . .

**EXHIBIT C-2**

Revisions	Date



**GROUND FLOOR**

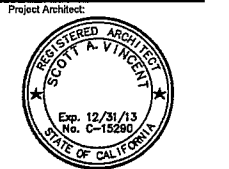


**SECOND FLOOR**

PROPOSED DEVELOPMENT FOR:  
**PACIFIC RIM  
COMPANIES**  
 WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA

**BUILDING STATISTICS**

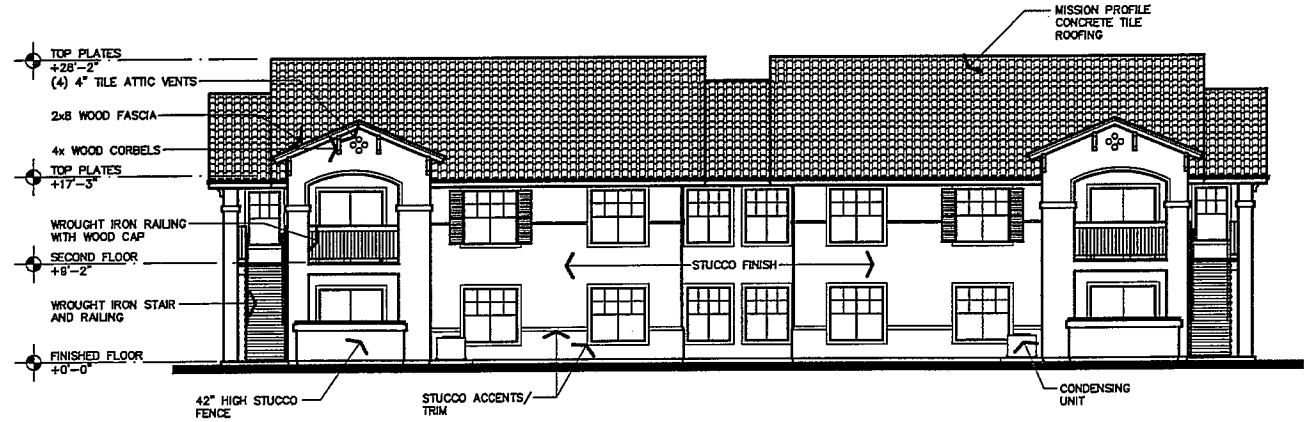
<b>BUILDING UNIT MIX</b>	(8) UNITS TYPE "3" 3 BEDROOMS 2 BATHROOMS WALK-UP UNIT 1066 SQ. FT./UNIT
<b>OCCUPANCY GROUP</b>	R-2
<b>CONSTRUCTION TYPE</b>	V-B WITH NFPA 13R AUTOMATIC FIRE SPRINKLER SYSTEM
<b>BUILDING AREA</b>	
a. GROUND FLOOR	4,284 SQ. FT.
1. RESIDENTIAL UNITS	462 SQ. FT.
2. COVERED PORCH/PATIOS	422 SQ. FT.
b. SECOND FLOOR	4,284 SQ. FT.
1. RESIDENTIAL UNITS	462 SQ. FT.
2. COVERED PORCH/PATIOS	422 SQ. FT.
<b>TOTAL BUILDING AREA</b>	8,512 SQ. FT.
<b>FIRE RESISTANCE REQUIREMENTS</b>	
a. EXTERIOR WALLS	NON-RATED
b. INTERIOR BEARING WALLS	NON-RATED
c. INTERIOR NON-BEARING WALLS	NON-RATED
d. CEILING/ROOF ASSEMBLY	NON-RATED
e. FLOOR/CEILING ASSEMBLY	WJ-1.6 ONE HOUR RATED FLOOR/CEILING PER ASTM E 119/NFPA 251
f. UNIT COMMON WALLS	ONE HOUR RATED (G.A. FILE NO. WF-3370)
g. SHAFT ENCLOSURES	NONE OCCUR
h. EXTERIOR OPENINGS	NON-RATED



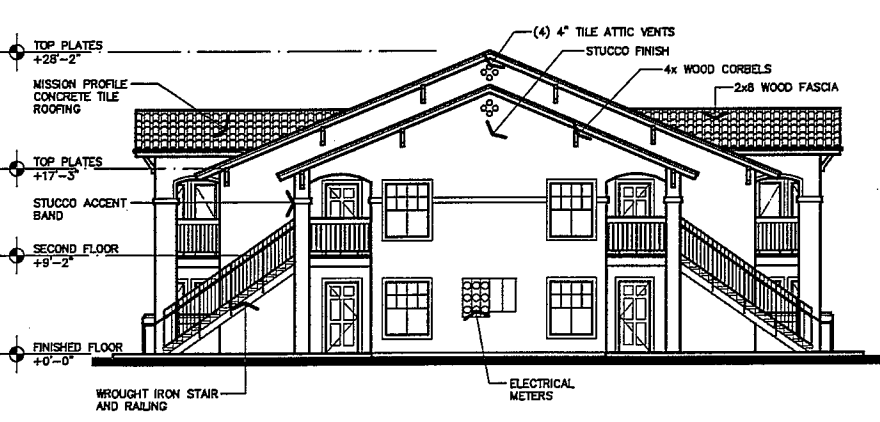
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Reference North:  
  
 Scale: 1/8" = 1'-0"  
 Project Name: PACIFIC RIM  
 PORTERVILLE, CA.  
 Project Number: 110025  
 Plot Date: 03.07.12  
 Sheet Number:



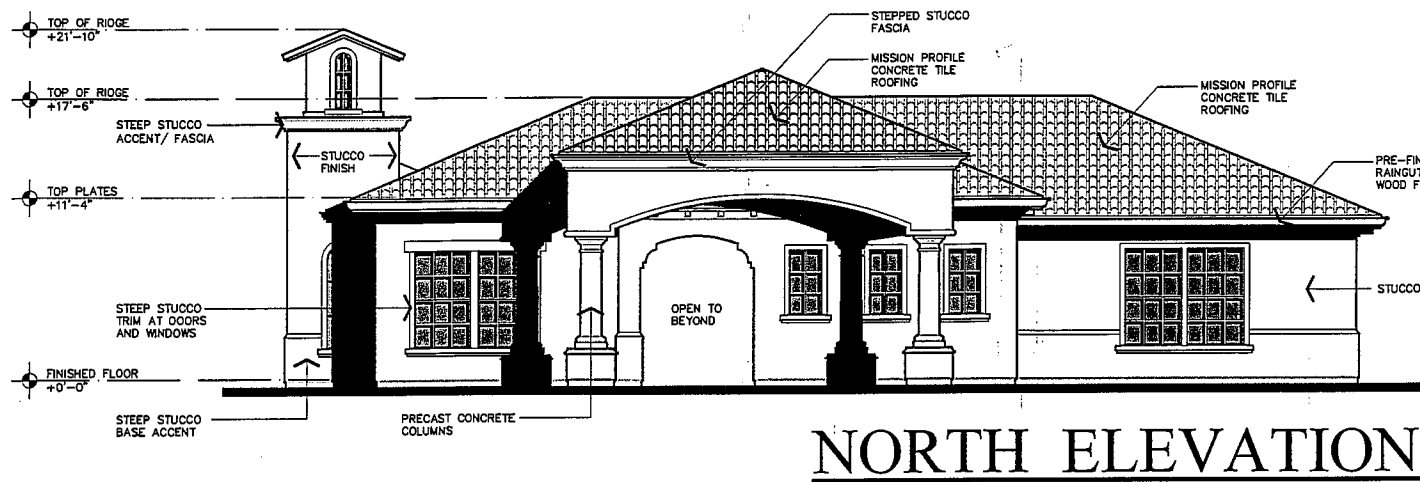
**FRONT ELEVATION**



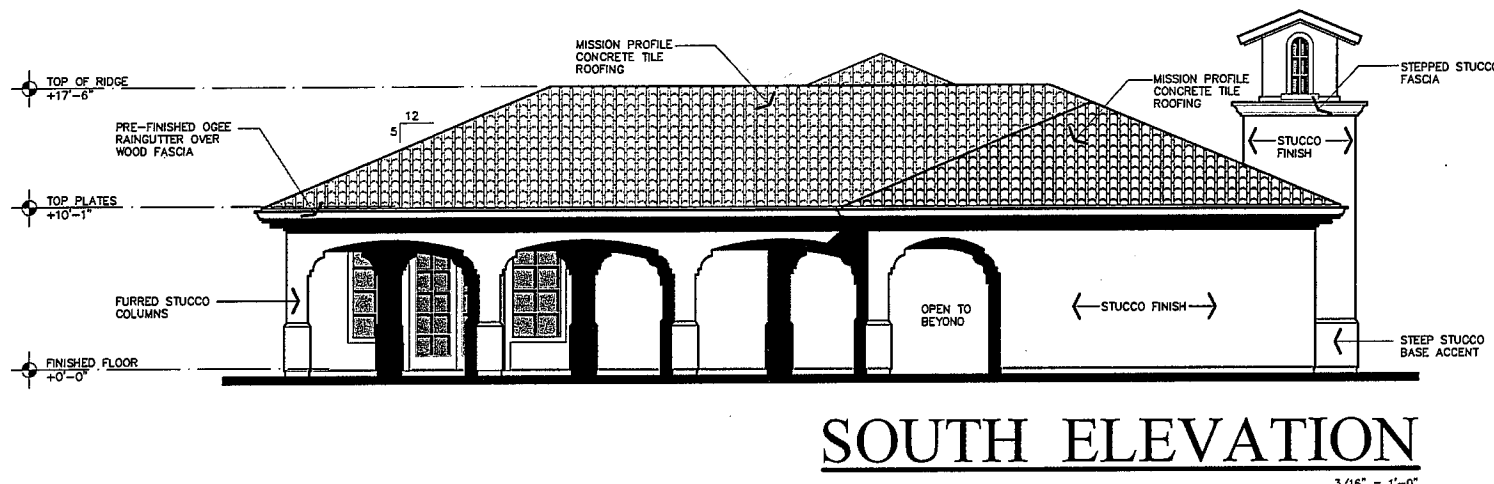
**SIDE ELEVATION**

**BUILDING C**

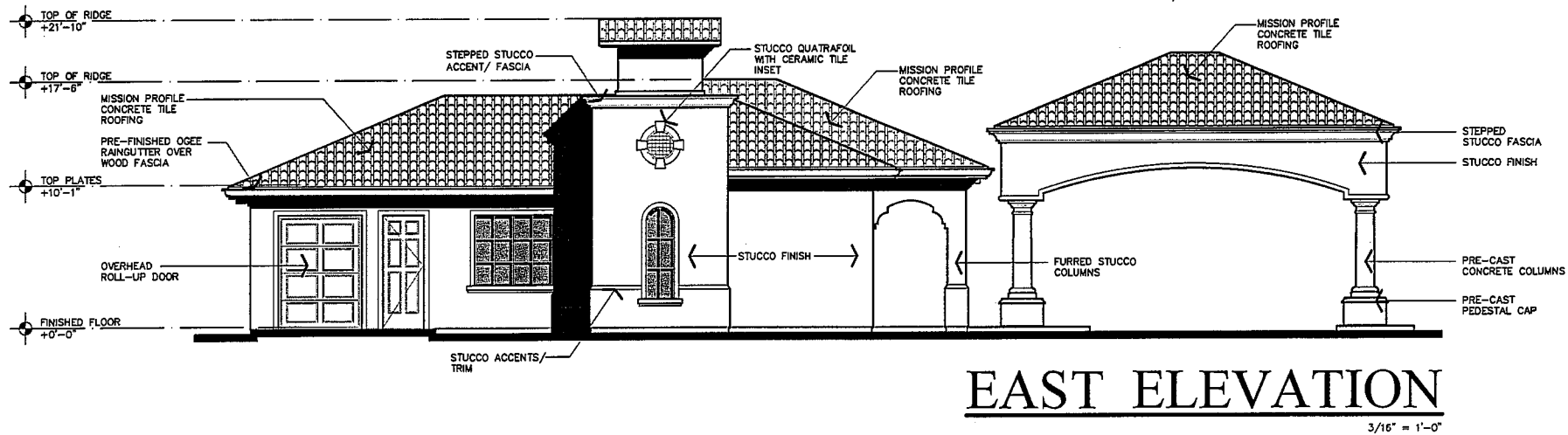
**EXHIBIT C-3**



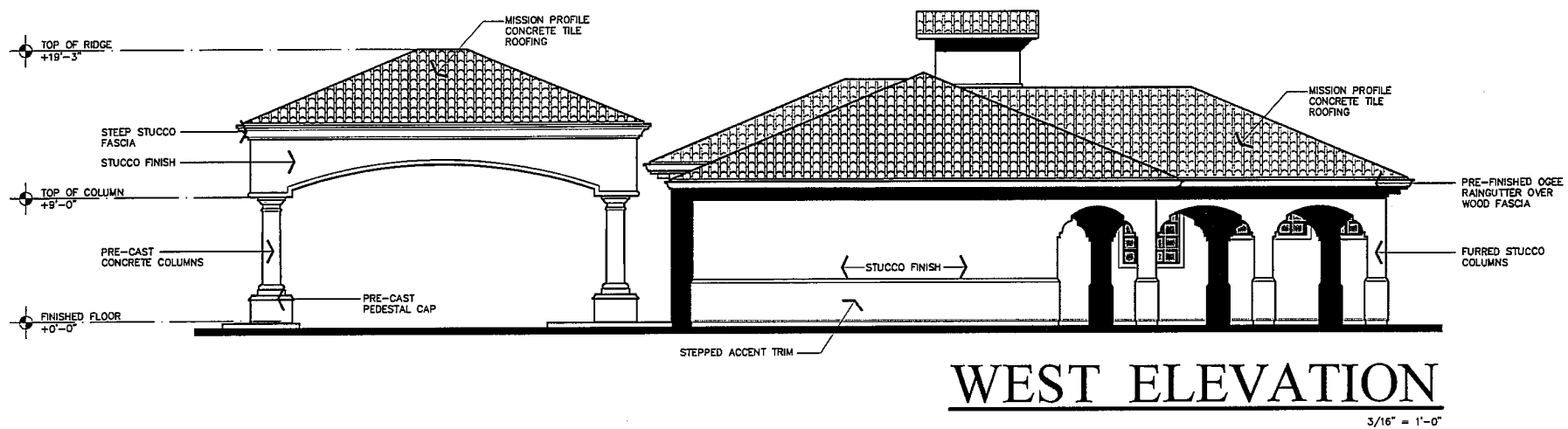
**NORTH ELEVATION**  
3/16" = 1'-0"



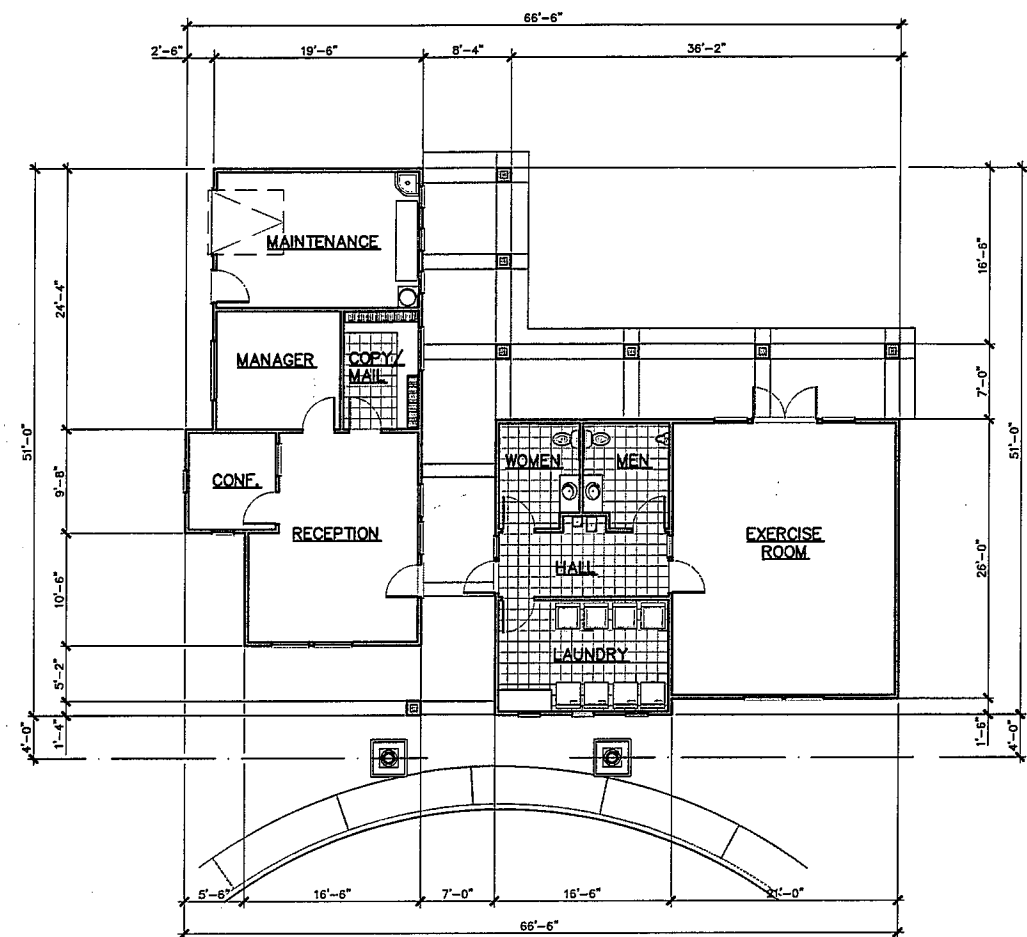
**SOUTH ELEVATION**  
3/16" = 1'-0"



**EAST ELEVATION**  
3/16" = 1'-0"



**WEST ELEVATION**  
3/16" = 1'-0"



PORTE COCHERE

**FLOOR PLAN** 1/8" = 1'-0"

**BUILDING STATISTICS**

OCCUPANCY GROUP	B
CONSTRUCTION TYPE	V-B
AREA CALCULATIONS	
a. RECEPTION AREA	302 SQ. FT.
b. CONFERENCE ROOM	84 SQ. FT.
c. MANAGER'S OFFICE	136 SQ. FT.
d. MAIL/ COPY ROOM	85 SQ. FT.
e. MAINTENANCE	253 SQ. FT.
f. TOILETS/ HALL	278 SQ. FT.
g. EXERCISE ROOM	546 SQ. FT.
h. LAUNDRY ROOM	176 SQ. FT.
j. PORTE COCHERE	626 SQ. FT.
k. COVERED PORCH	650 SQ. FT.
m. TOTAL BUILDING AREA	3,136 SQ. FT.
ALLOWABLE AREA	REFER TO ALLOWABLE AREA CALCULATIONS, SHEET T-4

**THE VINCENT COMPANY**

ARCHITECTS, INC.

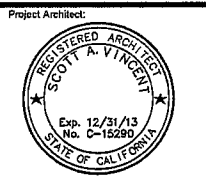
1500 West Shaw, Ste. 30  
Fresno, California 93711

Phone: 559.225.2602

Revisions Date

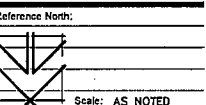
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PROPOSED MULTIFAMILY DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA



Issues:	Date:
DESIGN REVIEW	03.08.12
PLAN CHECK	
BACK CHECK	
PERMITS	
CONSTRUCTION	
AS BUILT	

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Scale: AS NOTED  
Project Name: PACIFIC RIM  
PORTERVILLE, CA.  
Project Number: 110925  
Plot Date: 03.08.12  
Sheet Number:

**COMMUNITY BUILDING**

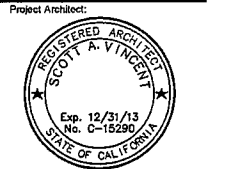
**PP1.8**

Of . . . Sheets . . .

**EXHIBIT C-4**

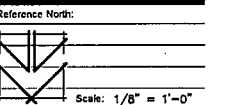
Revisions	Date

PROPOSED MULTI-FAMILY DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA



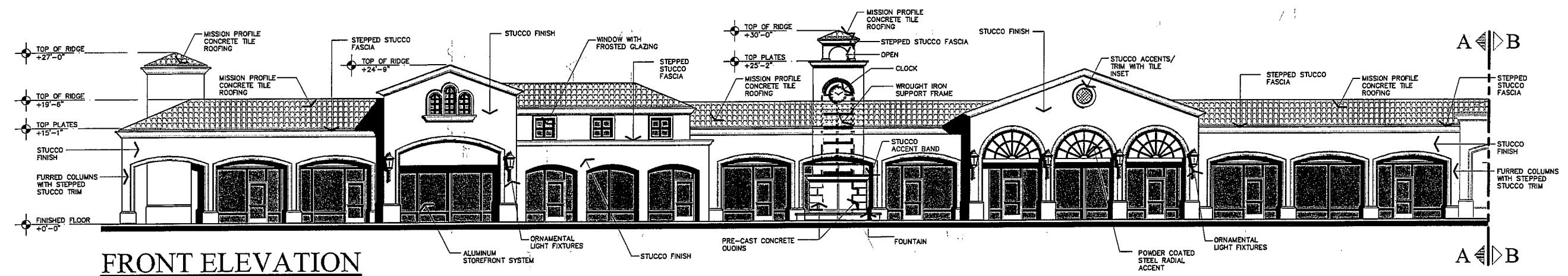
Issuances:	Date:
● DESIGN REVIEW	03.08.12
○ PLAN CHECK	
○ BACK CHECK	
○ PERMITS	
○ CONSTRUCTION	
○ AS BUILT	

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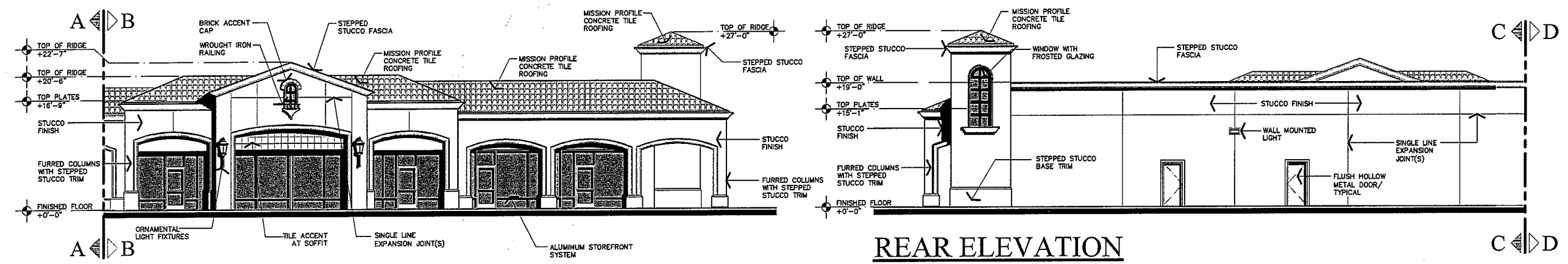


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Project Name: PACIFIC RIM  
PORTERVILLE, CA  
Project Number: 110925  
Plot Date: 10.12.12  
Sheet Number:

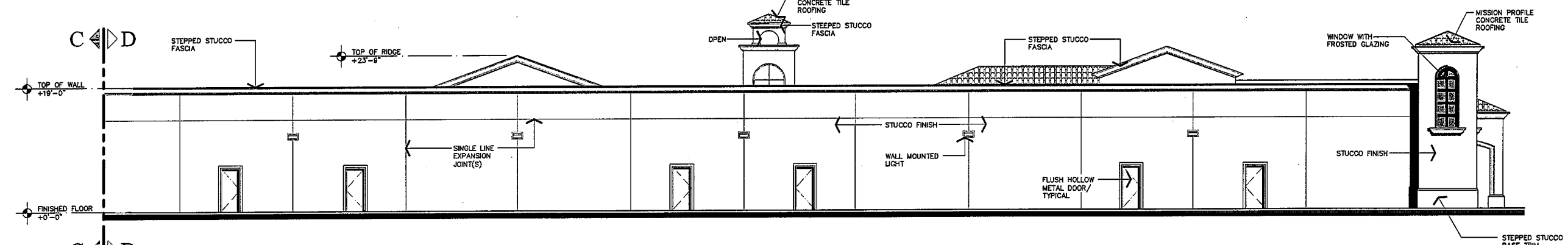
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Of . . . Sheets . . .



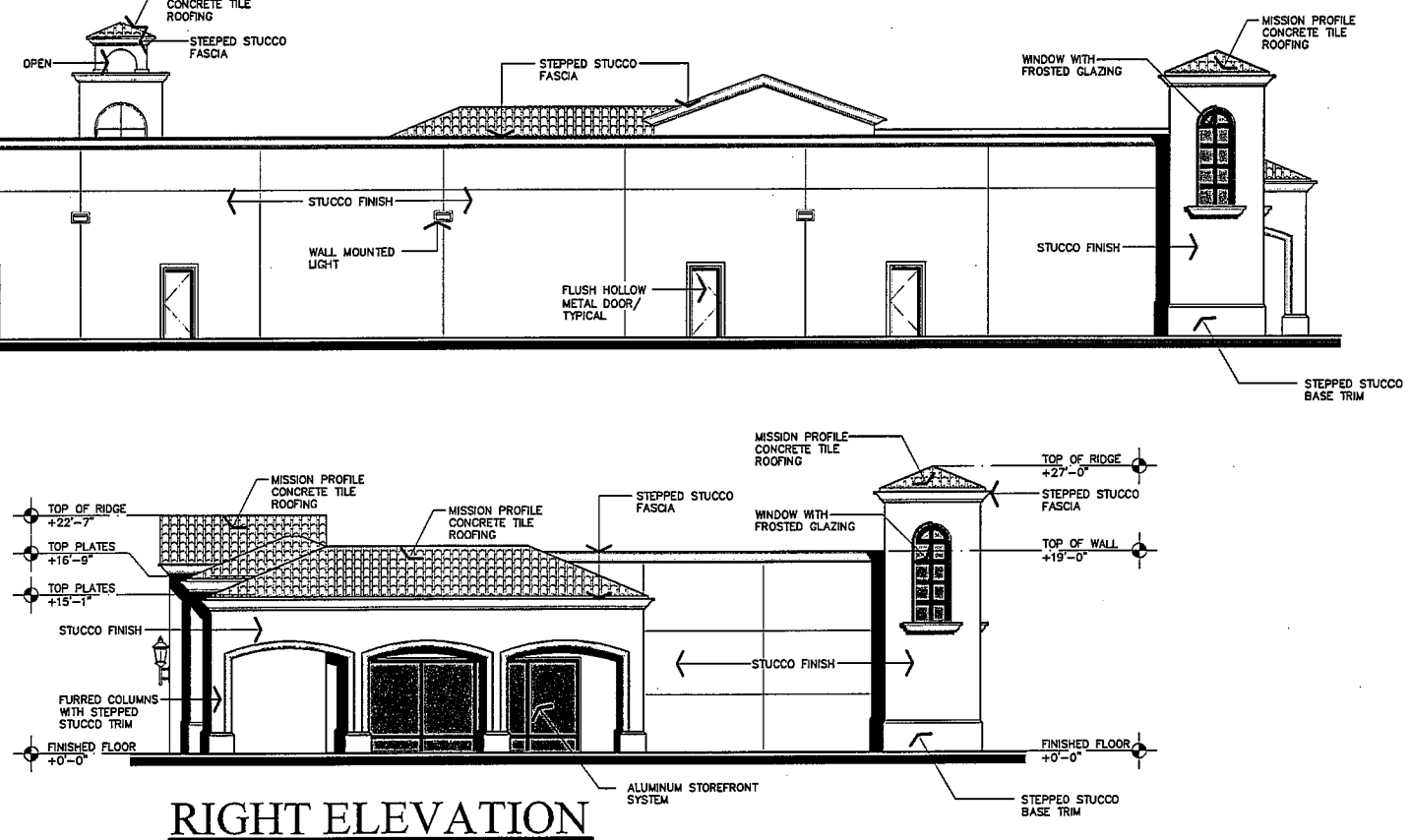
**FRONT ELEVATION**



**REAR ELEVATION**



**LEFT ELEVATION**



**RIGHT ELEVATION**

**RETAIL 'A' ELEVATIONS**

1/8" = 1'-0"

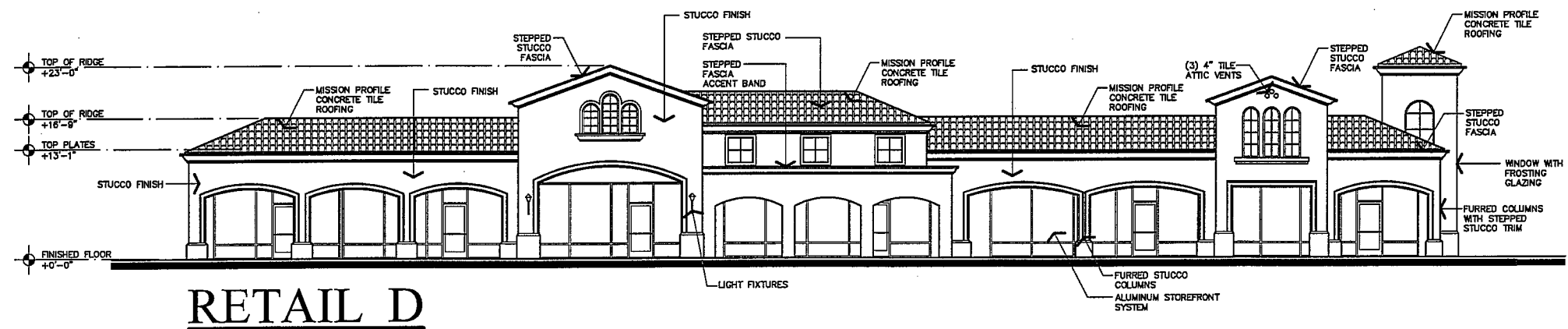
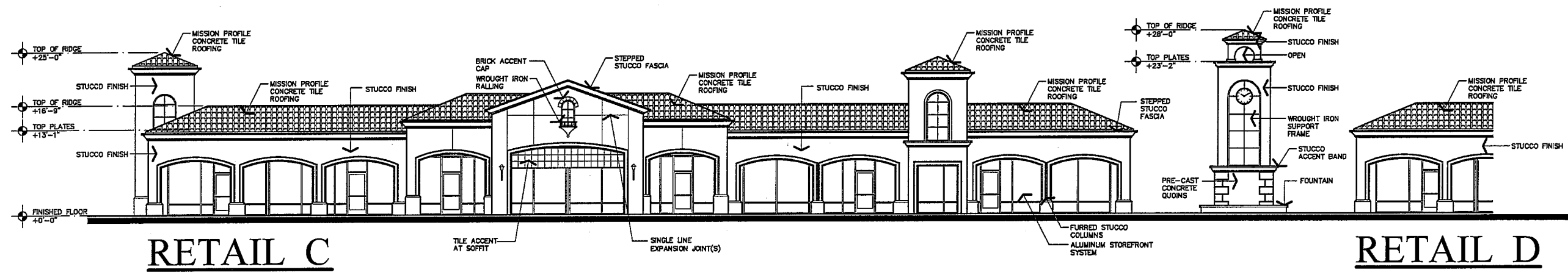
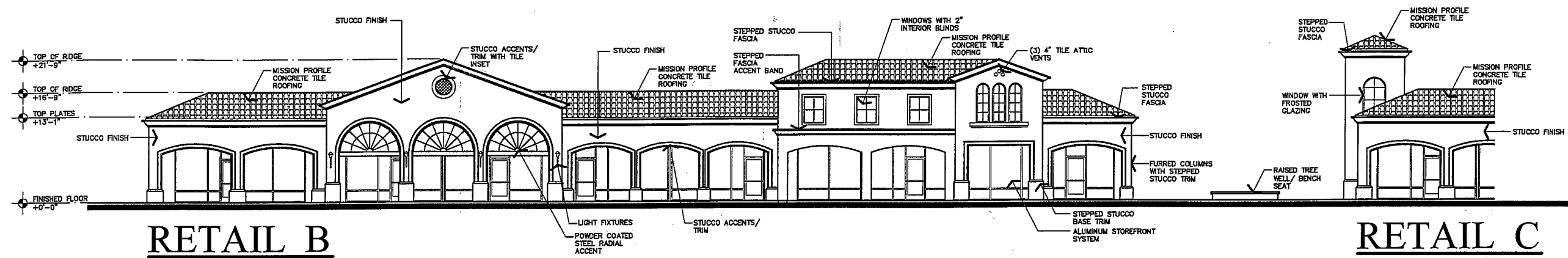
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**EXHIBIT C-5**

**THE VINCENT COMPANY**  
**ARCHITECT'S, INC.**  
 1500 West Shaw, Ste. 30  
 Fresno, California 93711  
 Phone: 559.225.2602

Revisions

Revisions	Date



RETAIL B

RETAIL C

RETAIL C

RETAIL D

RETAIL D

**PROPOSED DEVELOPMENT FOR:**  
**PACIFIC RIM COMPANIES**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA

Project Architect:



Issues:	Date:
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<input type="checkbox"/> PLAN CHECK	
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<input type="checkbox"/> AS BUILT	

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Reference North:

Scale: 1/8" = 1'-0"

Project Name: PACIFIC RIM  
 PORTERVILLE, CA.

Project Number: 110925  
 Plot Date: 03.07.12

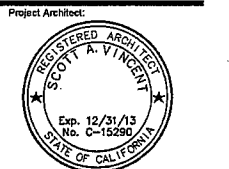
Sheet Number:  
**PP2.2**  
 Of . Sheets .

RETAIL CENTER  
 ELEVATIONS  
 1/8" = 1'-0"

**EXHIBIT C-6**

Revisions	Date

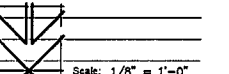
PROPOSED MULTIFAMILY DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA



Issues:	Date:
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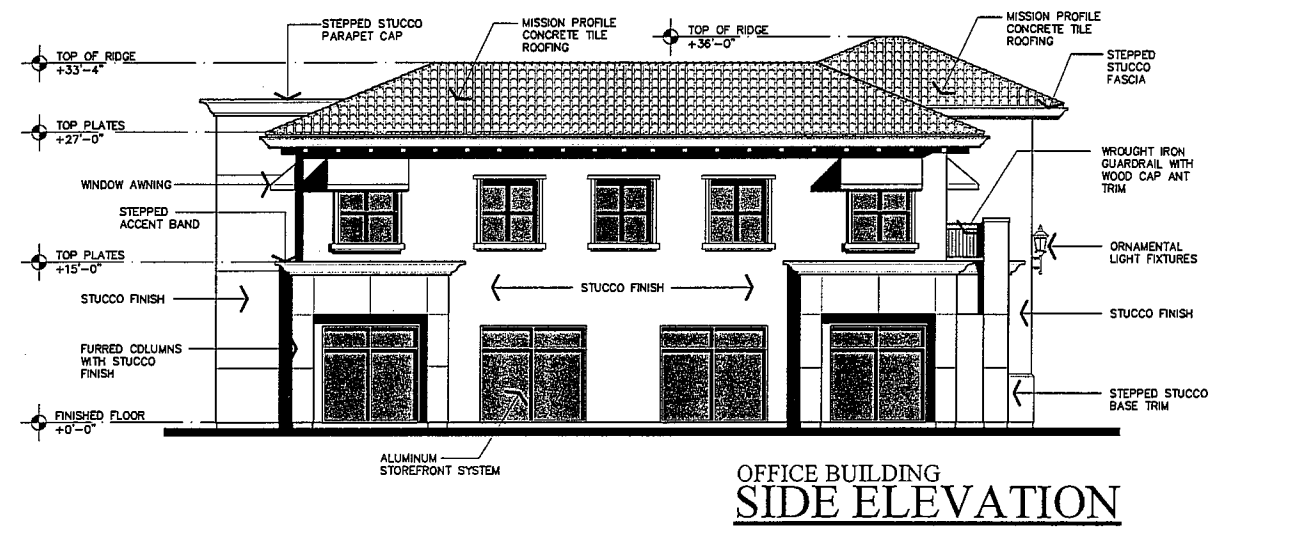
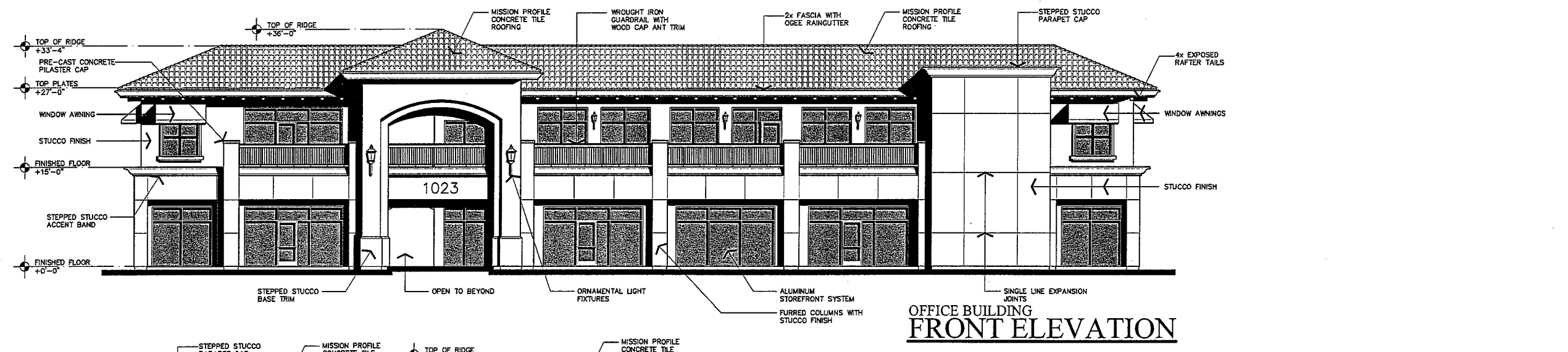
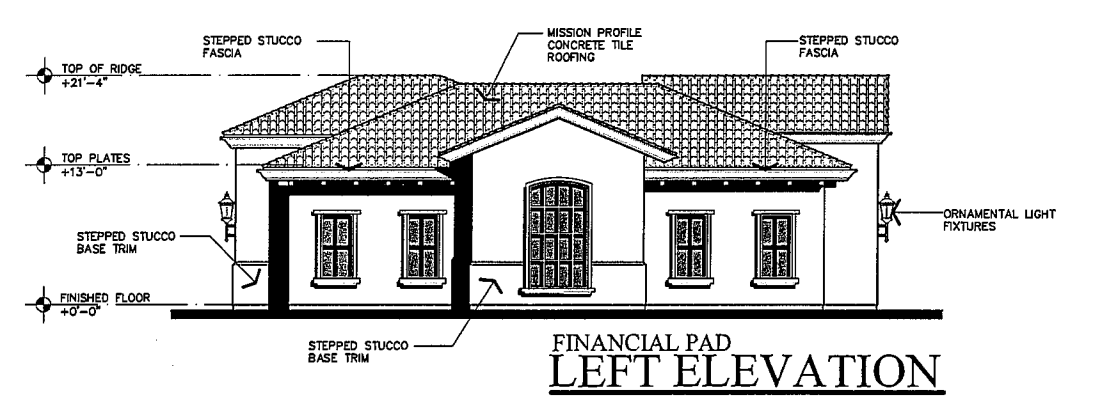
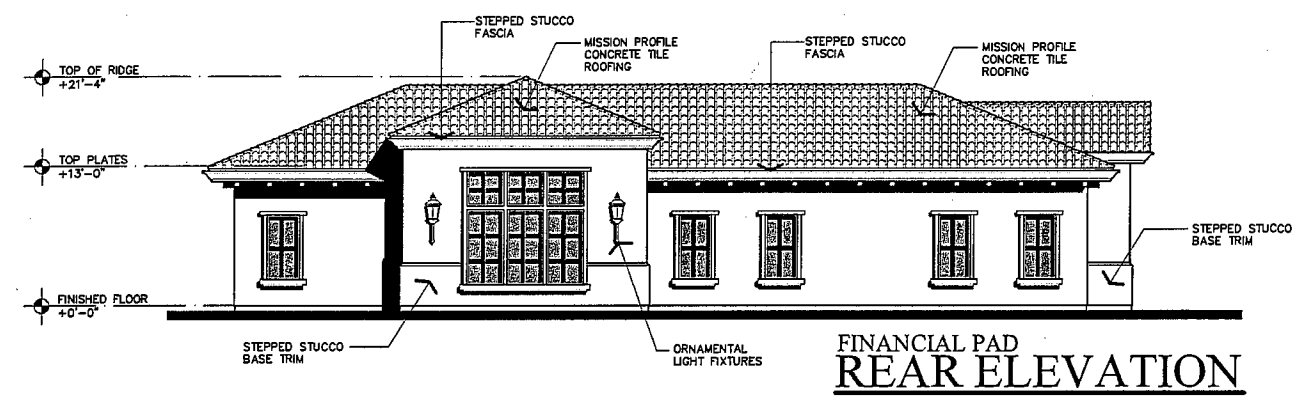
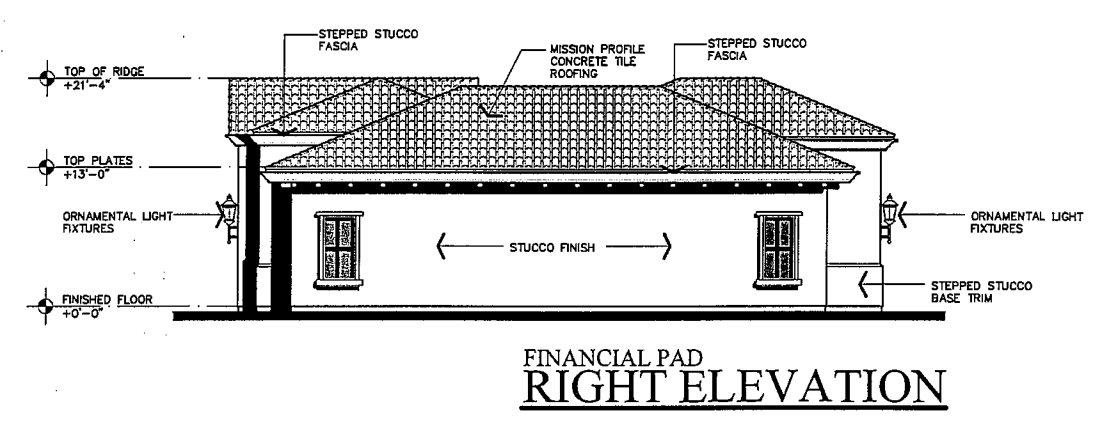
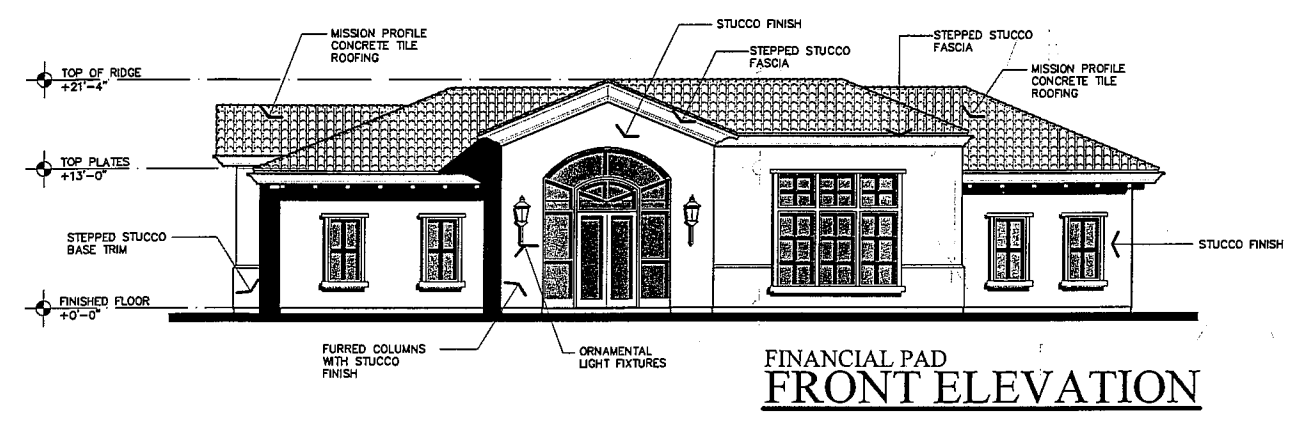
Reference North:



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PORTERVILLE, CA.  
Project Number: 110925  
Plot Date: 10.17.12  
Sheet Number:

**PP2.4**  
Of . . . Sheets . . .

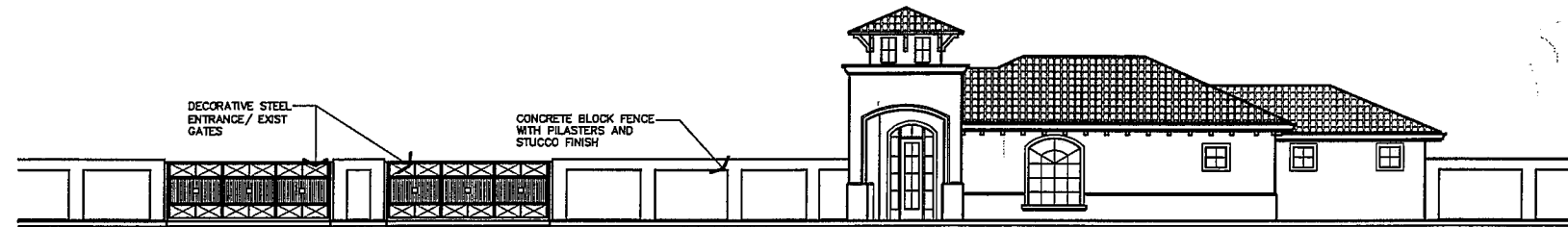
EXHIBIT C-7



**EXTERIOR ELEVATIONS** 1/8" = 1'-0"

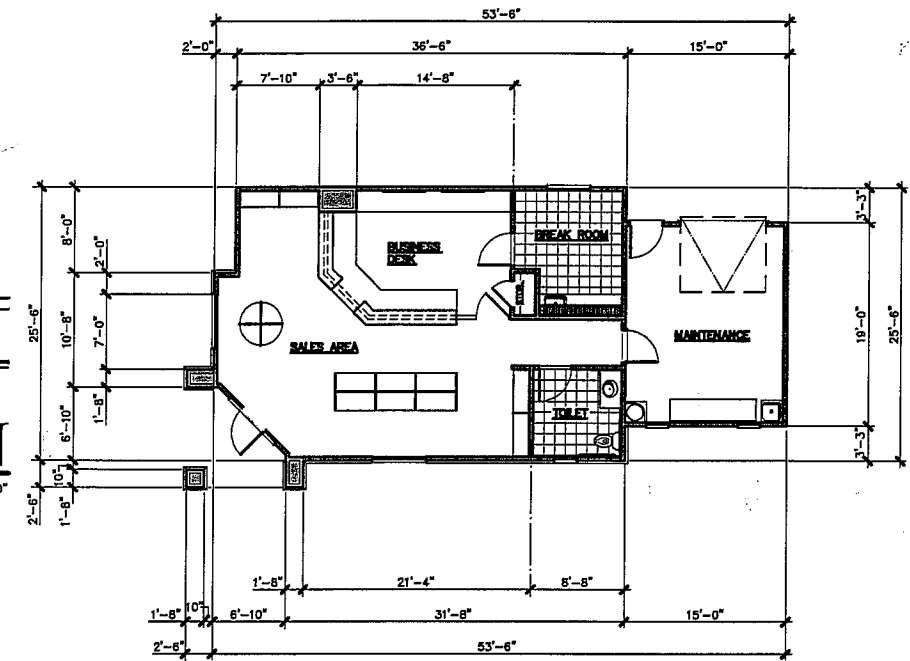
Z:\01111025 - Porterville Apartments\DESIGN PROPOSALS\Site Plan Review\PP2\_ARCH\EXTERIOR ELEVATIONS.dwg 10/22/2012 12:23 AM Amy

Revisions	Date



**ENTRANCE ELEVATION**

1/8" = 1'-0"



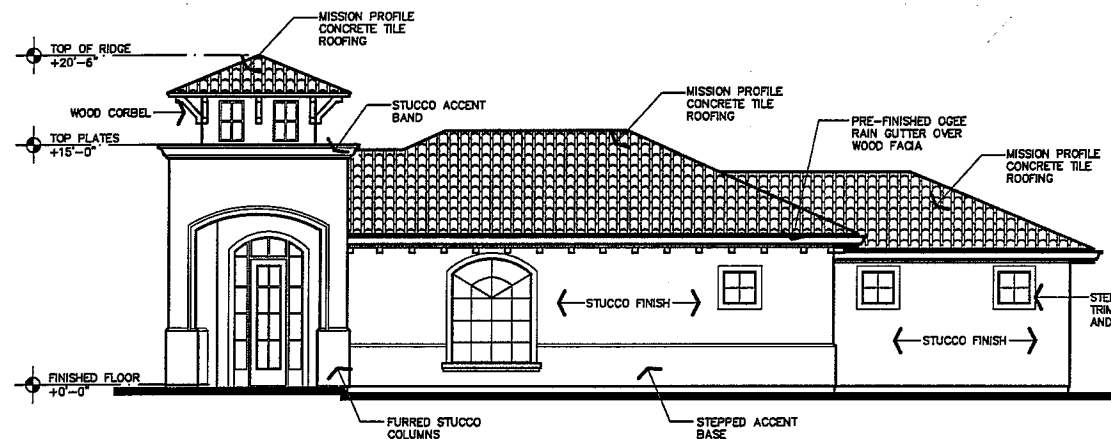
**FLOOR PLAN**

1/8" = 1'-0"

**BUILDING STATISTICS**

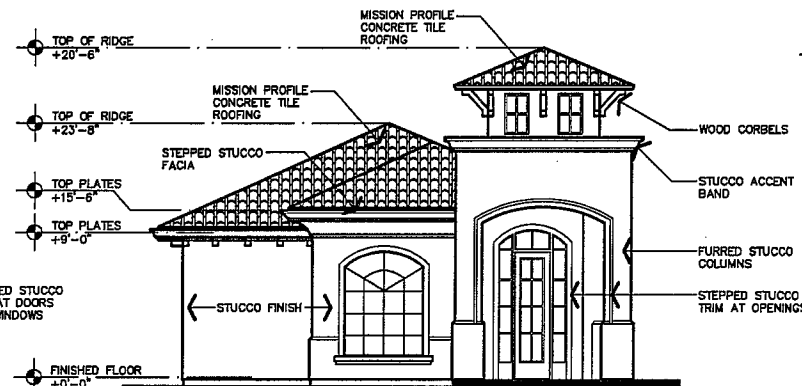
OCCUPANCY GROUP	B
CONSTRUCTION TYPE	V-B
AREA CALCULATIONS	
a. SALES AREA	478 SQ. FT.
b. BUSINESS DESK	209 SQ. FT.
c. BREAK ROOM	114 SQ. FT.
d. TOILET/ HALL/ STORAGE	135 SQ. FT.
e. MAINTENANCE	288 SQ. FT.
f. COVERED PORCH	86 SQ. FT.
g. TOTAL BUILDING AREA	1,290 SQ. FT.

PROPOSED DEVELOPMENT FOR:  
**PACIFIC RIM COMPANIES**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA



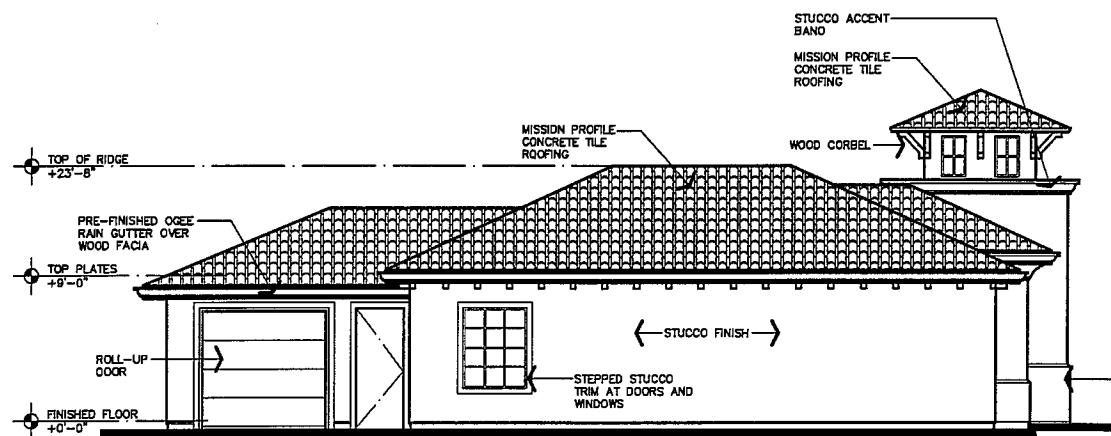
**NORTH ELEVATION**

3/16" = 1'-0"



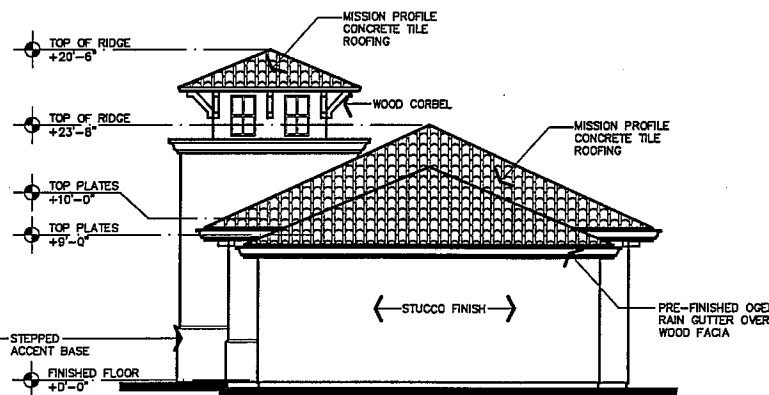
**EAST ELEVATION**

3/16" = 1'-0"



**SOUTH ELEVATION**

3/16" = 1'-0"



**WEST ELEVATION**

3/16" = 1'-0"

**MINI-STORAGE RENTAL OFFICE**

**PP3.2**

Revisions:	Date:
<input checked="" type="checkbox"/> DESIGN REVIEW	03.08.12
<input type="checkbox"/> PLAN CHECK	
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<input type="checkbox"/> AS BUILT	
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Reference North:	
Scale: AS NOTED	
Project Name: PACIFIC RIM	
PORTERVILLE, CA.	
Project Number: 110025	
Plot Date: 03.07.12	
Sheet Number:	
Of .	Sheets .

**EXHIBIT C-8**

# LANDSCAPE NOTES








**VINCENT COMPANY**  
ARCHITECT'S, INC.

1500 West Shaw, Ste. 30  
Fresno, California 93711  
Phone: 559.225.2602

Revisions	Date

- LANDSCAPE SUBCONTRACTOR SHALL BE RESPONSIBLE FOR SUBMISSION OF IRRIGATION PLANS TO THE ARCHITECT AND CITY OF PORTERVILLE BUILDING AND INSPECTION DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION OF THE SYSTEM. PLANS SHALL INCLUDE LOCATION AND TYPE OF ALL HEADS; LOCATION, MATERIALS AND SIZE OF ALL WATER LINES AND CALCULATION VERIFYING ADEQUACY OF WATER SUPPLY.
- PLANTING SHALL BE PERFORMED BY WORKMEN FAMILIAR WITH PLANTING PROCEDURES UNDER THE SUPERVISION OF A QUALIFIED PLANTING FOREMAN. THE PLANTING FOREMAN SHALL BE ON-SITE AT ALL TIMES WHEN PLANTING IS IN PROGRESS.
- PLANTS SHALL BE NURSERY GROWN, TYPICAL OF THEIR SPECIES AND VARIETY, AND SHALL BE FREE FROM DEFECTS AND INJURIES. QUALITY AND SIZE OF PLANTS, SPREAD OF ROOTS, AND SIZE OF BALLS SHALL BE IN ACCORDANCE WITH ANSI Z60.1-1969 "AMERICAN STANDARD FOR NURSERY STOCK"
- LAWN SEED SHALL BE FRESH STOCK, 95% PURE LIVE SEED BY WEIGHT. SEED SHALL PASS A GERMINATING TEST WITH 85% GERMINATION.
- LARGE TREES SHALL BE PLANTED FIRST. SHRUB PLANTING SHALL BE COMPLETED BEFORE GROUND COVER AND LAWN AREAS ARE PLANTED.
- ALL TREES, PLANTS AND GRASS PROVIDED UNDER THIS CONTRACT SHALL BE IN GOOD HEALTHY AND FLOURISHING CONDITION ONE GROWING YEAR FROM DATE OF ACCEPTANCE. EXCEPT FOR LOSS BEYOND CONTROL OF CONTRACTOR, REPLACEMENT OF TREES AND PLANTS OF COMPARABLE QUALITY AND SIZE SHALL BE MADE BY THE CONTRACTOR.
- EXTRA CARE SHALL BE TAKEN TO PRESERVE EXISTING MATURE LANDSCAPING. ANY PLANT MATERIAL DAMAGED DURING CONSTRUCTION SHALL BE REPLACED WITH NEW MATERIAL OF LIKE MATURITY AND QUALITY.
- ALL LANDSCAPING SHALL BE IN PLACE PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY.
- NO EXPOSED UTILITY BOXES, TRANSFORMERS, METERS, PIPING (EXCEPTING THE BACKFLOW PREVENTION DEVICE) ETC. ARE ALLOWED TO BE LOCATED IN THE LANDSCAPED AREAS OR SETBACKS OR ON THE STREET FRONTS OF THE BUILDINGS. ALL TRANSFORMERS, ETC. ARE TO BE SHOWN ON THE SITE PLAN. THE BACKFLOW PREVENTION DEVICE SHALL BE SCREENED BY LANDSCAPING OR OTHER SUCH MEANS AS MAY BE APPROVED.
- TREES SHALL BE MAINTAINED IN GOOD HEALTH. HOWEVER, TREES MAY NOT BE TRIMMED OR PRUNED TO REDUCE THE NATURAL HEIGHT OR OVERALL CROWN OF THE TREE, EXCEPT AS NECESSARY FOR THE HEALTH OF THE TREE AND PUBLIC SAFETY; OR AS MAY BE OTHERWISE APPROVED BY THE PUBLIC WORKS DEPARTMENT.
- PRIOR TO FINAL INSPECTION, A WRITTEN CERTIFICATION, SIGNED BY A LANDSCAPE PROFESSIONAL APPROVED BY THE DIRECTOR, SHALL BE SUBMITTED STATING THAT THE REQUIRED LANDSCAPING AND IRRIGATION SYSTEM WAS INSTALLED IN ACCORDANCE WITH THE LANDSCAPE AND IRRIGATION PLANS APPROVED BY THE PLANNING DIVISION, DEVELOPMENT DEPARTMENT.
- THE PROPERTY OWNER WILL PLANT REQUIRED STREET TREES - A \$30 PER TREE INSPECTION FEE MUST BE PAID AND A DEVELOPER STREET TREE PLANTING AGREEMENT MUST BE SIGNED PRIOR TO ISSUANCE OF BUILDING PERMITS.

## SYMBOLS

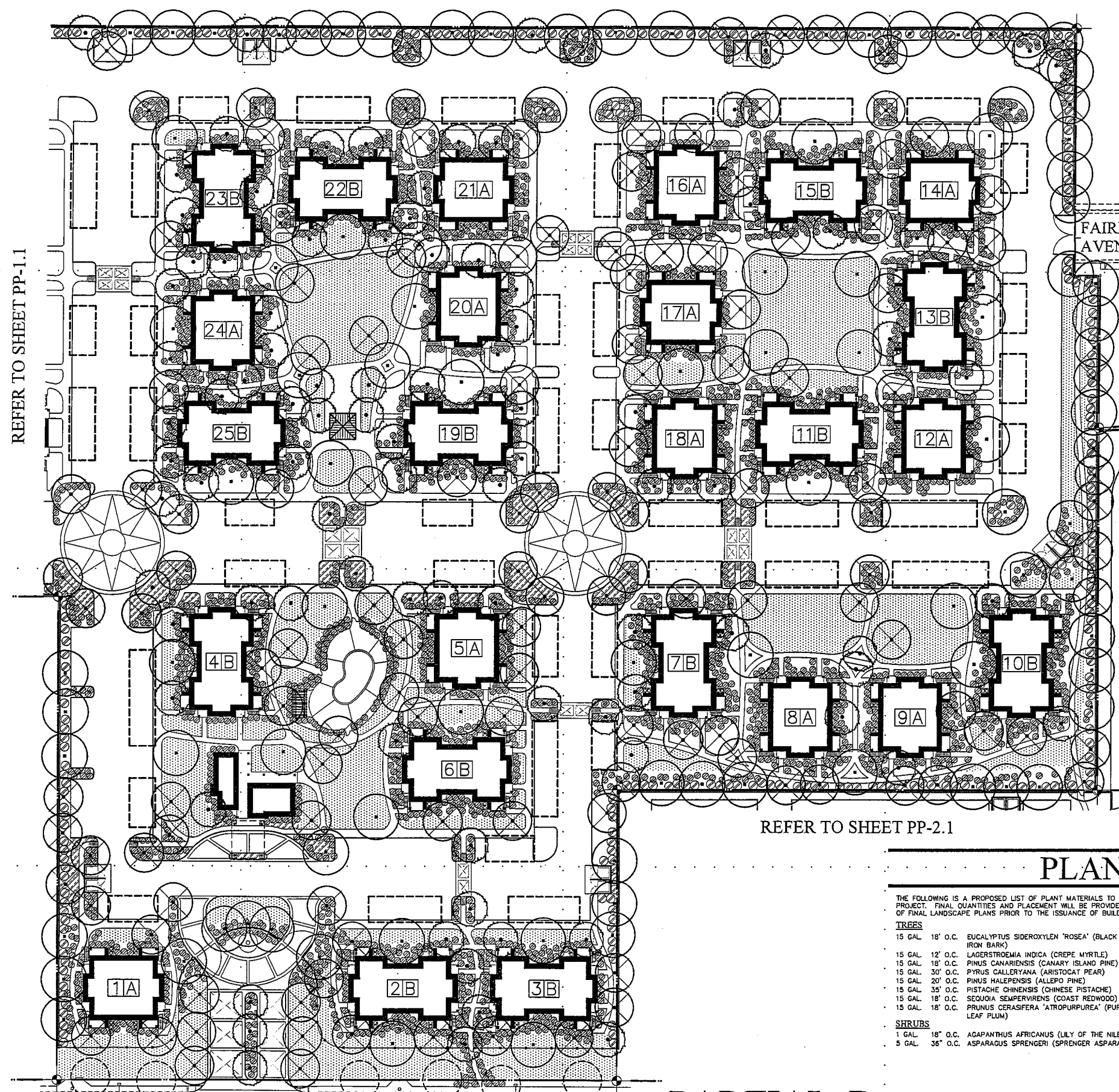
-  **STREET TREES**  
SPECIES TO BE SELECTED BY THE CITY OF PORTERVILLE PUBLIC WORKS DEPARTMENT
-  **LARGE TREES (30 - 35 FOOT CANOPY)**  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
-  **MEDIUM TREES (25 - 30 FOOT CANOPY)**  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
-  **SMALL TREES (20 - 25 FOOT CANOPY)**  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
-  **SHRUBS / PERENNIALS**  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
-  **GROUND COVER / ANNUALS**  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
-  **LAWN**

## PLANT MATERIALS

THE FOLLOWING IS A PROPOSED LIST OF PLANT MATERIALS TO BE UTILIZED ON THIS PROJECT. FINAL QUANTITIES AND PLACEMENT WILL BE PROVIDED WITH THE SUBMITTAL OF FINAL LANDSCAPE PLANS PRIOR TO THE ISSUANCE OF BUILDING PERMITS.

- TREES**
- 15 GAL 18" O.C. EUCALYPTUS SIDEROXYLEN 'ROSEA' (BLACK IRON BARK)
  - 15 GAL 12" O.C. LAGERSTROEMIA INDICA (CREPE MYRTLE)
  - 15 GAL 18" O.C. PINUS CANARIENSIS (CANARY ISLAND PINE)
  - 15 GAL 30" O.C. PYRUS CALLERYANA (ARISTOCAT PEAR)
  - 15 GAL 20" O.C. PINUS HALEPENSIS (ALLEPO PINE)
  - 15 GAL 35" O.C. PISTACHE CHINENSIS (CHINESE PISTACHE)
  - 15 GAL 18" O.C. SEQUOIA SEMPERVIRENS (COAST REDWOOD)
  - 15 GAL 18" O.C. PRUNUS CERASIFERA 'ATROPURPUREA' (PURPLE LEAF PLUM)
- SHRUBS**
- 1 GAL 18" O.C. AGAPANTHUS AFRICANUS (LILY OF THE NILE)
  - 5 GAL 36" O.C. ASPARAGUS SPRENGERI (SPRENGER ASPARAGUS)

- 5 GAL 36" O.C. AZALEA SOUTHERN INDICA (SUN AZALEA)
  - 5 GAL 36" O.C. CAMELLIA SASANGUA (SUN CAMELLIA)
  - 1 GAL 20" O.C. FICUS PUMILA (CREEPING FIG VINE)
  - 5 GAL 36" O.C. GAMPPELUS CHRYSANTHEOIDES (NO COMMON NAME)
  - 5 GAL 36" O.C. JUNIPERUS SABINA TAMARISCIFOLIA (DWARF TAMARISK)
  - 5 GAL 36" O.C. MAHONIA AQUIFOLIUM (CHINESE GRAPE)
  - 5 GAL 36" O.C. NANDINA DOMESTICA (HEAVENLY BAMBOO)
  - 5 GAL 72" O.C. PHOTENIA FRASERII (PHOTENIA)
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  - FLATS 12" O.C. ANNUALS (PETUNIAS, MUMS AND DWARF MARIGOLDS)



REFER TO SHEET PP-1.1

FAIRHAVEN AVENUE

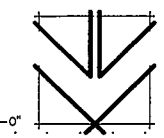
REFER TO SHEET PP-3.1

REFER TO SHEET PP-2.1

HENDERSON AVENUE

# PARTIAL B LANDSCAPE PLAN


1" = 40'-0"



PROPOSED DEVELOPMENT FOR:  
**PACIFIC RIM COMPANIES**  
 WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA



- Issuances:  DESIGN REVIEW 03.06.12  
 PLAN CHECK  
 BACK CHECK  
 PERMITS  
 CONSTRUCTION  
 AS BUILT
- All drawings and written material appearing herein constitute original work of the architect and may not be duplicated, used, or disclosed without written consent of the architect.

Reference North:  
  
 Scale: 1" = 40'-0"  
 Project Name: PACIFIC RIM  
 PORTERVILLE, CA  
 Project Number: 110925  
 Plot Date: 03.06.12  
 Sheet Number:

**PL1.2**  
 Of . . . Sheets . . .

EXHIBIT D-1



# LANDSCAPE NOTES

**VINCENT COMPANY**  
ARCHITECT'S, INC.

1500 West Shaw, Ste. 30  
Fresno, California 93711

Phone: 559.225.2602

Revisions	Date

- LANDSCAPE SUBCONTRACTOR SHALL BE RESPONSIBLE FOR SUBMISSION OF IRRIGATION PLANS TO THE ARCHITECT AND CITY OF PORTERVILLE BUILDING AND INSPECTION DEPARTMENT FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION OF THE SYSTEM. PLANS SHALL INCLUDE LOCATION AND TYPE OF ALL HEADS; LOCATION, MATERIALS AND SIZE OF ALL WATER LINES AND CALCULATION VERIFYING ADEQUACY OF WATER SUPPLY.
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- ALL TREES, PLANTS AND GRASS PROVIDED UNDER THIS CONTRACT SHALL BE IN GOOD HEALTHY AND FLOURISHING CONDITION ONE GROWING YEAR FROM DATE OF ACCEPTANCE. EXCEPT FOR LOSS BEYOND CONTROL OF CONTRACTOR, REPLACEMENT OF TREES AND PLANTS OF COMPARABLE QUALITY AND SIZE SHALL BE MADE BY THE CONTRACTOR.
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- PRIOR TO FINAL INSPECTION, A WRITTEN CERTIFICATION, SIGNED BY A LANDSCAPE PROFESSIONAL APPROPRIATE TO THE PROJECT, SHALL BE SUBMITTED STATING THAT THE REQUIRED LANDSCAPING AND IRRIGATION SYSTEM WAS INSTALLED IN ACCORDANCE WITH THE LANDSCAPE AND IRRIGATION PLANS APPROVED BY THE PLANNING DIVISION, DEVELOPMENT DEPARTMENT.

PROPOSED DEVELOPMENT FOR:

**PACIFIC RIM COMPANIES**

WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA

Project Architect:



Issuance:	Date:
<input checked="" type="checkbox"/> DESIGN REVIEW	03.08.12
<input type="checkbox"/> PLAN CHECK	
<input type="checkbox"/> BACK CHECK	
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<input type="checkbox"/> CONSTRUCTION	
<input type="checkbox"/> AS BUILT	

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Reference North:

Scale: 1"=40.00'

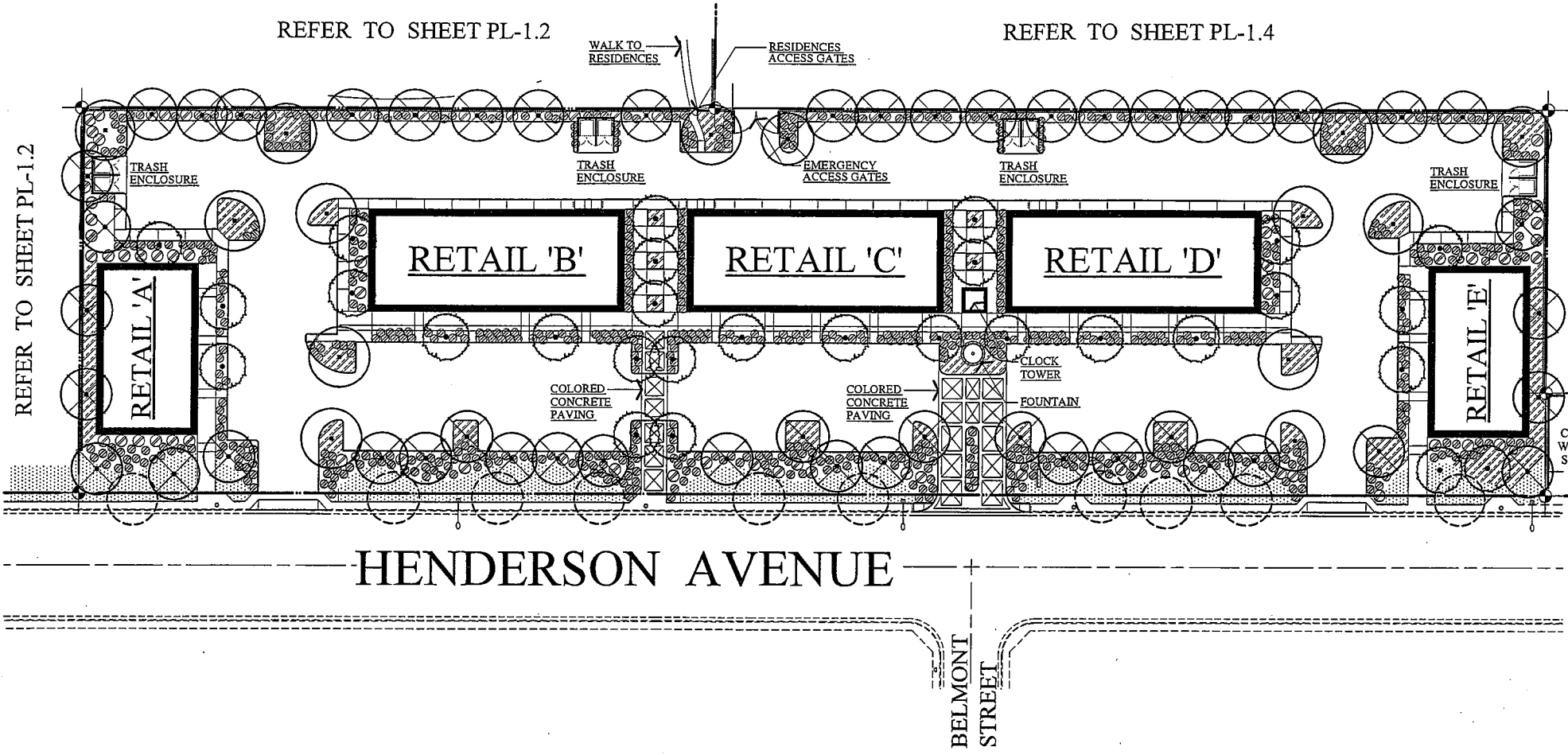
Project Name: PACIFIC RIM  
PORTERVILLE, CA.

Project Number: 110925  
Plot Date: 03.05.12

Sheet Number:

**PL1.3**

Of . . . Sheets . . .



## RETAIL CENTER LANDSCAPE PLAN

### SYMBOLS

- STREET TREES  
SPECIES TO BE SELECTED BY THE CITY OF PORTERVILLE PUBLIC WORKS DEPARTMENT
- LARGE TREES (30 - 35 FOOT CANOPY)  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
- MEDIUM TREES (25 - 30 FOOT CANOPY)  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
- SMALL TREES (20 - 25 FOOT CANOPY)  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
- SHRUBS / PERENNIALS  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
- GROUND COVER / ANNUALS  
SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
- LAWN

### PLANT MATERIALS

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- TREES**
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  - FLATS 12" O.C. ANNUALS (PETUNIAS, MUMS AND DWARF MARIGOLDS)

EXHIBIT D-2

# LANDSCAPE NOTES TO THE

**VINCENT COMPANY**

ARCHITECT'S, INC.  
1500 West Shaw, Ste. 30  
Fresno, California 93711

Phone: 559.225.2602

Revisions	Date

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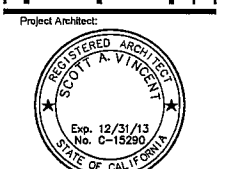
PROPOSED DEVELOPMENT FOR:  
**PACIFIC RIM COMPANIES**  
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA

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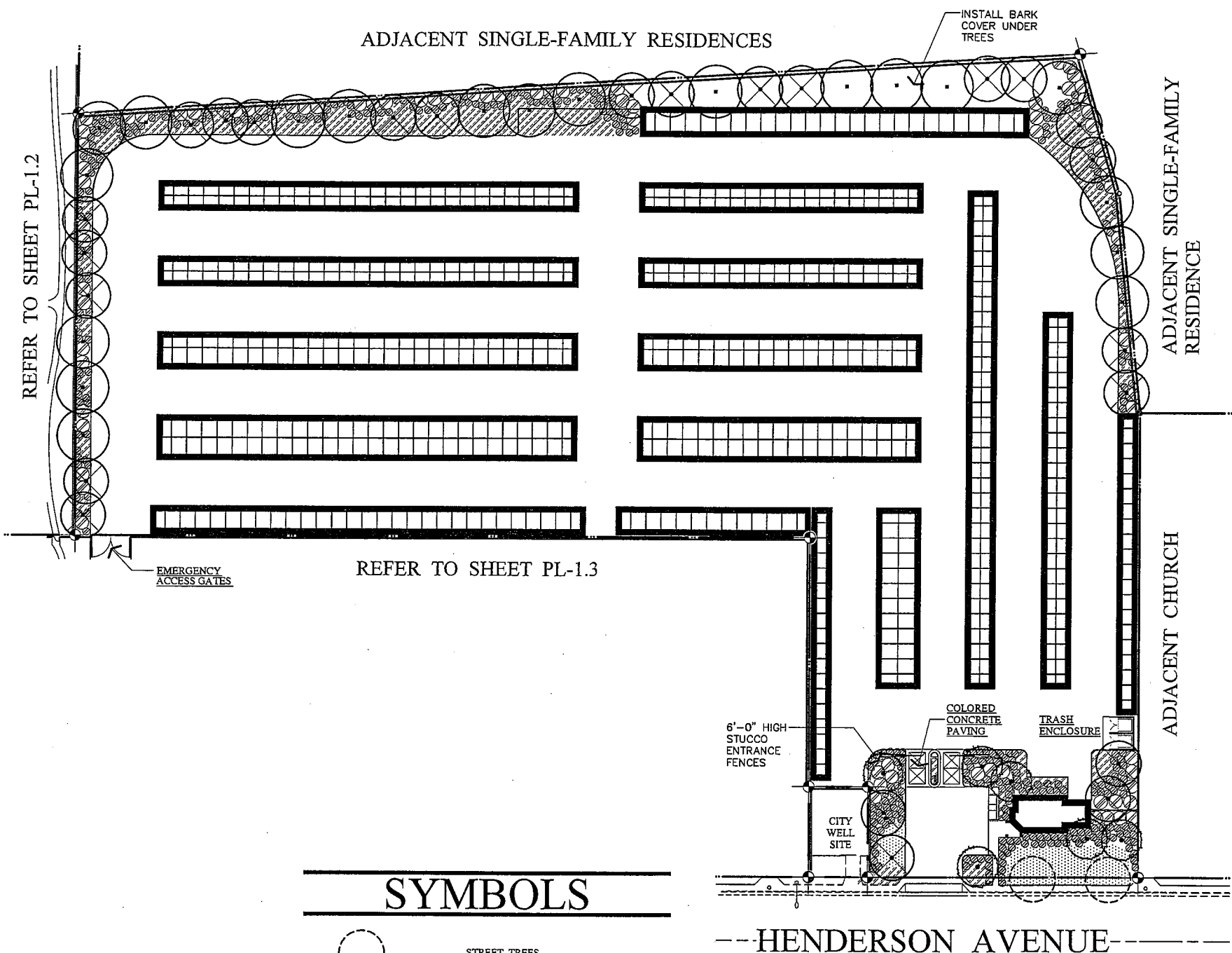


Issuance:	Date:
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<input type="checkbox"/> CONSTRUCTION	
<input type="checkbox"/> AS BUILT	

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Reference North:  
Scale: 1" = 40.00'  
Project Name: PACIFIC RIM  
PORTERVILLE, CA.  
Project Number: 110925  
Plot Date: 03.07.12  
Sheet Number:

**PL1.4**  
Of . . . Sheets . . .



## SYMBOLS

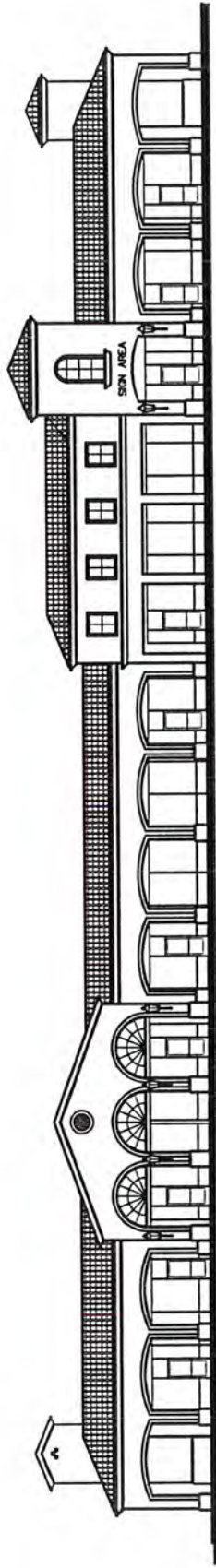
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SELECTED FROM THE LIST OF PROPOSED PLANT MATERIALS
  - LAWN**
- 1 CONDENSING UNIT SCREENING & CLEARANCES**

# MINI-STORAGE LANDSCAPE PLAN

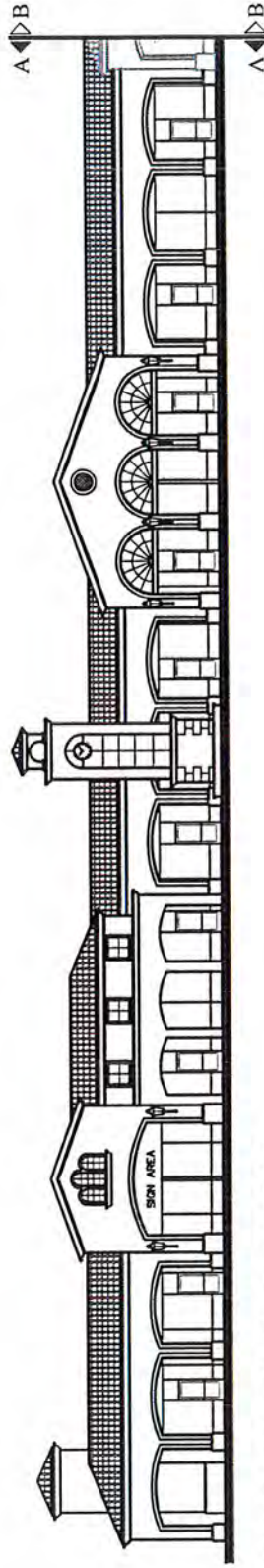
1" = 40'-0"

EXHIBIT D-3

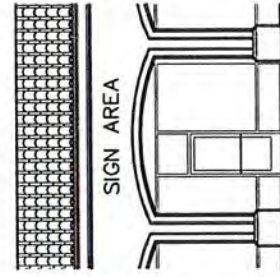
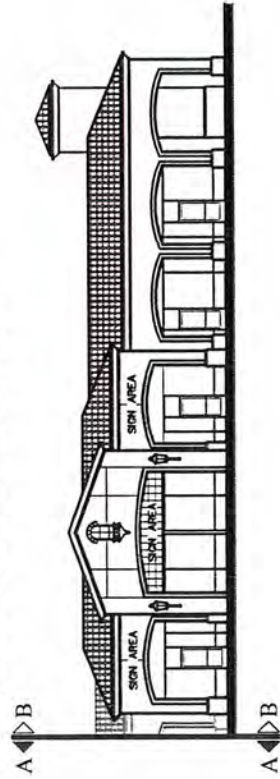
# SIGN LOCATION EXHIBIT "E"



FRONT ELEVATION

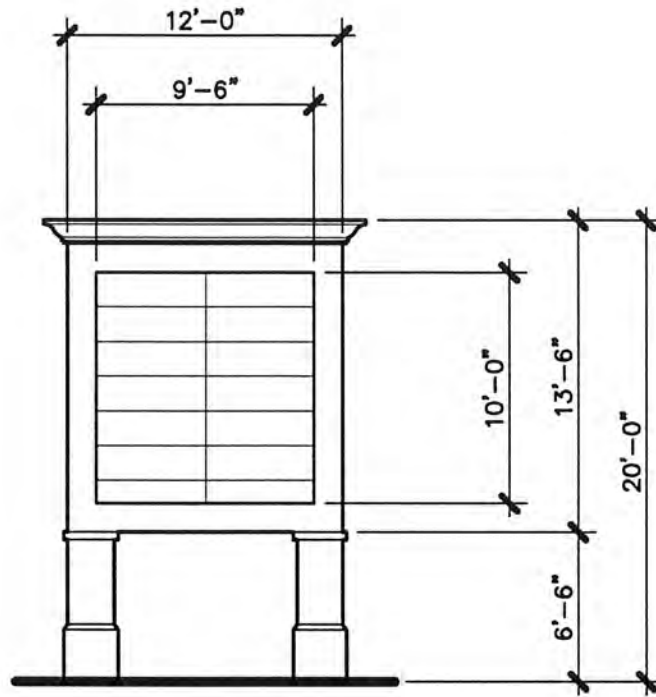


FRONT ELEVATION

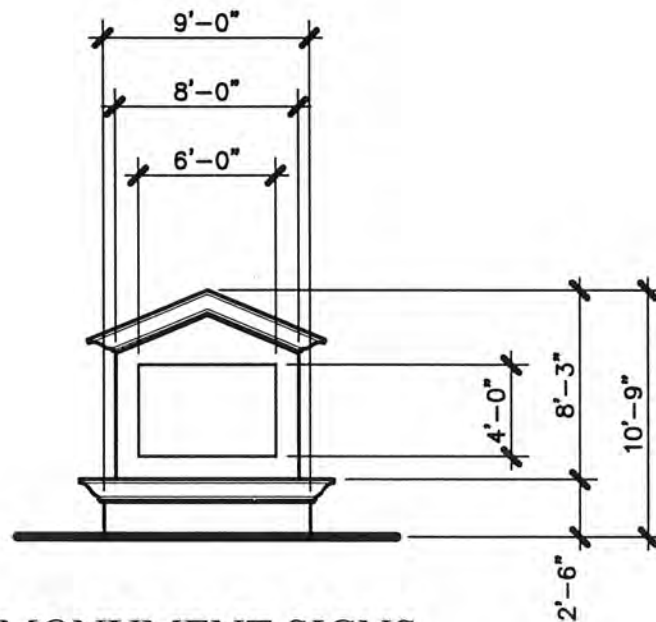


TYPICAL

# EXHIBIT " " "

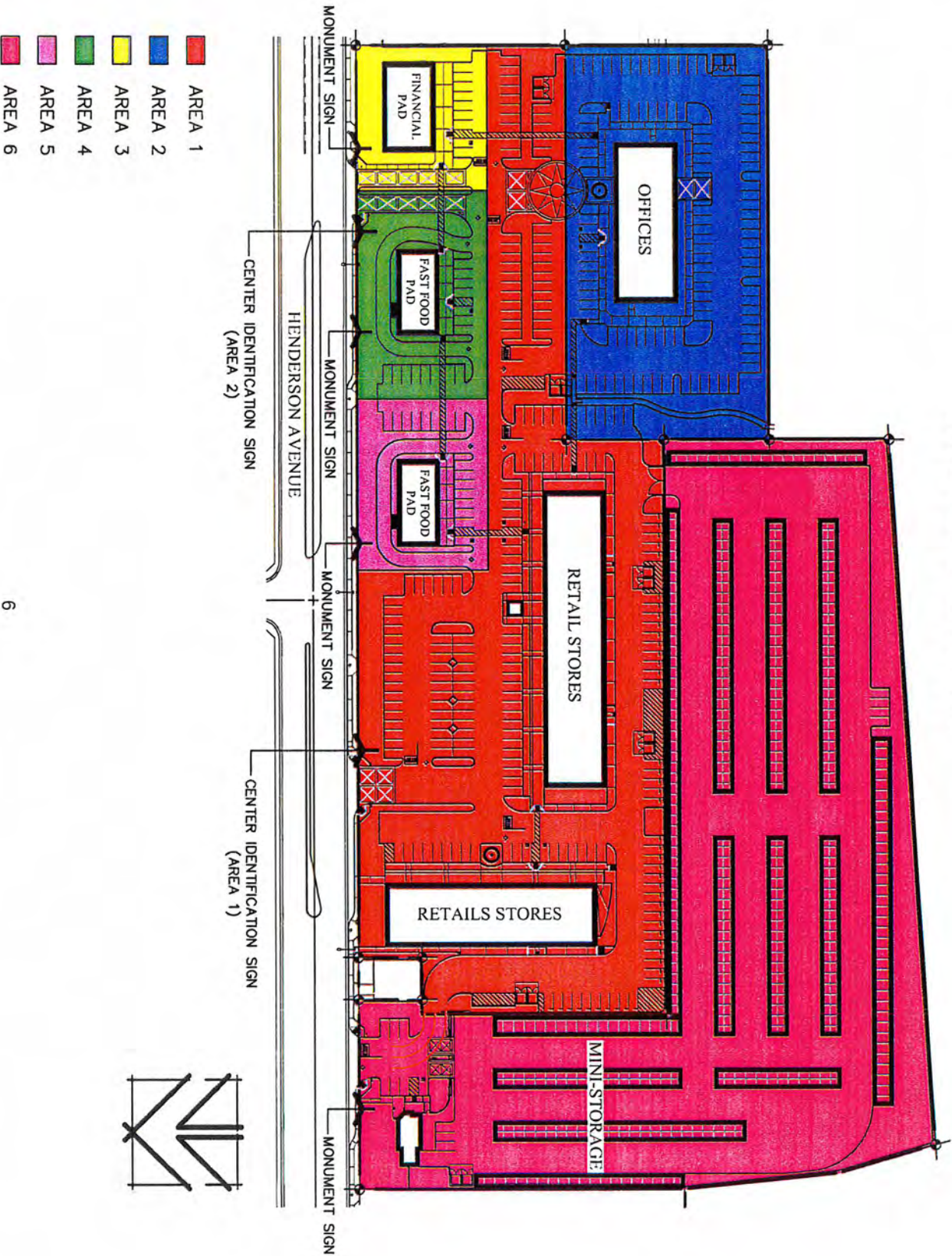


CENTER IDENTIFICATION SIGNS  
(AREAS 1 AND 2)



MONUMENT SIGNS  
(AREAS 3, 4, 5 AND 6)

# SITE PLAN EXHIBIT " "



**City of Porterville**  
**(development name) Master Sign Program**  
**Henderson Avenue**  
**MSP-**

This Master Sign Program applies to the (development name). This master sign program has been established for the purpose of assuring an outstanding shopping center for the community and for the mutual benefit of all tenants. Conformance may be strictly enforced by the City, and any nonconforming or unapproved signs must be brought into conformance at the expense of the applicable tenant.

**A. GENERAL REQUIREMENTS**

1. Tenants shall be responsible for installation and maintenance of all tenant's individual signs at the tenant's expense.
2. Tenants shall be responsible for the fulfillment of all requirements and specifications of this Criteria.
3. Each tenant shall submit to the owner, before fabrication, at least two (2) copies of detailed drawings of proposed signs, indicating the location on the building and on the property, size of sign, layout, design and color of the proposed sign including all lettering and/or graphics, and a detailed method of attachment. However such detailed drawings may be submitted electronically.
4. All tenants at their own expense are responsible for obtaining a Porterville City Sign Permit. Both the tenant's and landlord's signature are required on all sign applications. Fabrication prior to City approval is not recommended. Said Sign Permit must be obtained by a State of California licensed contractor.
5. Tenants must have their signs, as specified in this Criteria installed within 90 days of opening for business. Signs must be removed within 15 days after their departure. Tenants shall be responsible for any necessary repairs to the building fascia, (i.e.) patching, painting etc. caused during installation or removal of the signs. All expenses for such shall be the responsibility of and paid for by the tenant.

**B. GENERAL SPECIFICATION**

1. All tenant signs shall be internally lighted individual channel letters as specified in Section "C" of this Criteria.
2. All tenant signs must be installed within the designated sign area on the building as shown in Exhibit A, and as specified in paragraphs 3 and 4 of this Section "B".

3. Major tenants may display their company name or agency, logo, products, departments or services that are consistent with their national branding of their store fronts or office buildings. A maximum of two (2) square feet of sign, per each linear foot of tenant's leasehold frontage will be allowed, not to exceed 350 square feet, however no single sign shall exceed 300 square feet in area. Total linear feet of the signs shall not exceed 75% in horizontal length of the linear frontage of each tenant's leasehold frontage.
4. All other tenants' signs shall be displayed on one (1) line of text to include any logo or business name, with a minimum height of 12 inches and a maximum height of 24 inches. Two lines or "stacked" text or logos shall not be permitted. The overall size of all tenant signs shall not exceed two (2) square feet of sign per linear foot of tenant's leasehold frontage. No sign shall exceed more than 75% in horizontal length of the linear frontage of the tenant leasehold space.
5. Tenants of areas 3, 4 and 5 as designated on Exhibit "C" may display their company name or agency, logo, products, departments or services that are consistent with their national branding of their stores or offices.
6. All signs are to be installed centered on the arched opening nearest to the main entrance door of the respective tenant's leasehold (see Exhibit "A").
7. At the property owner's sole discretion, depending on the space available, tenants may be afforded a space on the Center Identification sign, located on Henderson Avenue (Exhibit "B"). No alterations to the spaces on these signs will be allowed without the landlord's prior written approval as provided for in Section A, paragraph 3. However, no alterations or additional signs on the exterior of these signs will be allowed.
8. No additional freestanding signs are allowed for tenants of areas 1 and 2 other than the Center Identification signs on Henderson Avenue as shown on Exhibit "B". Monument signs shall be allowed on Areas 3-6 as identified on Exhibit "C" as shown on Exhibit "B".
9. Design, layout and materials for tenant signs shall conform in all respects with the sign design drawings included within these criteria. The maximum height for letters in the body of all signs shall be as indicated in this document.
10. All signage within this shopping center is subject to Chapter 305:12 "Master Sign Program" of the Porterville Development Ordinance. However, at the Zoning Administrator's discretion, Chapter 305 "signs" shall prevail if there are any discrepancies or omissions within this Master Sign Program.

#### C. CONSTRUCTION SPECIFICATIONS

1. Channel letter signs are to be manufactured using .040 aluminum returns, 1/8 inch plastic faces held on with 3/4 inch thick plastic trim capping. Illumination is to be provided using L.E.D. lighting.
2. Major tenants and tenants of areas 3, 4 and 5 (Exhibit "C") will have the option of lighting their signs using either L.E.D. or 15mm Neon Tubing.
3. Tenants will have their choice of color; however, they must be approved by the landlord.
4. All electrical wiring shall be installed and enclosed behind the wall where letters are installed. No exposed crossover wiring or exposed raceways on the front of the building will be allowed.

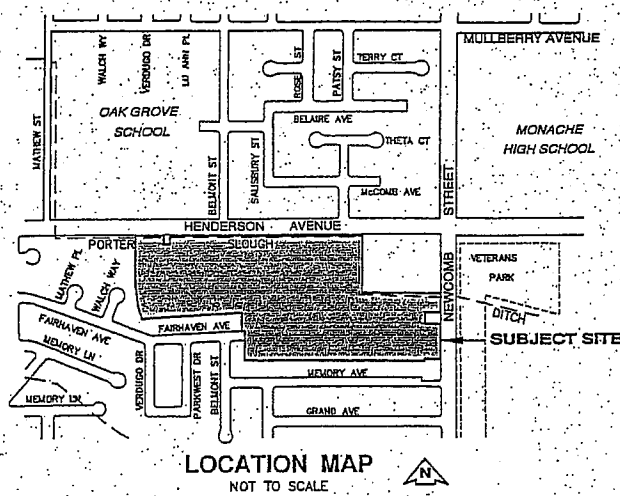
MSP - (development name) (Henderson Avenue)  
December 2012

5. Electrical wiring to the sign on the front fascia of the tenant's leasehold store front on the building to be existing. Tenants will be responsible at their sole expense for any timing device to operate the signs.
6. No audible, flashing or animated signs will be permitted.
7. All electrical wiring shall conform to and pass all state and local building codes.
8. All tenant signs shall be manufactured under Underwriters Laboratories specifications, and carry the U.L. label.

D. MISCELLANEOUS REQUIREMENTS

1. Advertising placards, banners, pendants, names, insignias, trademarks, or other descriptive materials, either temporary or permanently affixed or maintained upon the glass panes and supports of the show windows and doors, shall conform with the City of Porterville sign codes for such. Window signs shall not exceed 50% of the total glass window area. The above shall not be hand drawn.
2. Except as provided herein, no advertising placards, banners, pendants, names, insignia, trademarks or other descriptive materials shall be affixed or maintained upon the exterior walls or building without previous written approval of the landlord, and must receive a Temporary Sign Permit from the City of Porterville for such. The above shall not be hand drawn.
3. All sign plans must be submitted to the landlord for approval prior to the obtaining of a City of Porterville Sign Permit and installation as specified in Section "A", paragraph 3 of these Criteria.
4. The property owner reserves the right to designate the Sign Contractor to be contracted by the tenant, at the tenant's sole expense. Tenants must contact the property owner's agent as shown below to coordinate the hiring of the designated sign contractor. However, at the property owner's discretion, a tenant may be approved to use a Sign Contractor of their choice.
5. Any proposed Sign Contractor must be currently licensed as a C-45 State of California sign contractor. Said contractor must provide the following:
  - a. Certificate of Liability Insurance in the amount of \$2,000,000.00, naming \_\_, as additionally insured.
  - b. Certificates of The State of California Workman's Compensation.
6. Owners agent: (name): Phone: (phone number) e-mail: (email address)





- ### NOTES
1. DATE: AUGUST 15, 2012 REVISED JANUARY 29, 2013
  2. APN: 245-070-082, 245-410-032
  3. EXISTING USE: VACANT
  4. PROPOSED USE: MEDIUM DENSITY RESIDENTIAL AND COMMERCIAL
  5. DRAINAGE: MAINTAIN EXISTING PATTERNS, UNLESS OTHERWISE NOTED.
  6. DOMESTIC WATER: CITY OF PORTERVILLE
  7. SANITARY SEWER: CITY OF PORTERVILLE
  8. NATURAL GAS: THE GAS COMPANY
  9. POWER: SOUTHERN CALIFORNIA EDISON
  10. TELEPHONE: A.T. & T.
  11. CABLE TV: CHARTER COMMUNICATIONS
  12. FLOODING: ZONE X, PER FIRM
  13. PROPOSED IMPROVEMENTS, PUBLIC UTILITIES AND TIME OF COMPLETION: AS SHOWN OR AS REQUIRED BY CITY OF PORTERVILLE.

### LEGEND

—	PROPERTY BOUNDARY
—	SIGN
○	MANHOLE
○	POWER POLE
○	WATER METER
○	WATER VALVE
○	FIRE HYDRANT
○	STREET LIGHT
○	BUSH
○	TREE
—	PARCEL No. PER PARCEL MAP No. 2806

# TENTATIVE PARCEL MAP

BEING A DIVISION OF PARCEL 4 AND A PORTION OF PARCEL 3 OF PARCEL MAP No. 2806 SITUATED IN THE NORTH EAST 1/4 SECTION 28, T.21S., R.27E., M.D.B. & M. IN THE CITY OF PORTERVILLE, COUNTY OF TULARE, STATE OF CALIFORNIA.

For: **PAUL & SUSAN OWHADI**  
 1470 W. HERNDON AVENUE  
 FRESNO, CALIFORNIA 93711  
 (424) 234-5555

By: **JAMES WINTON & ASSOCIATES**  
 150 WEST HORTON AVENUE  
 PORTERVILLE, CALIFORNIA 93257  
 (559) 781-2700

### OWNER'S STATEMENT

WE HEREBY APPLY FOR APPROVAL OF THE DIVISION OF REAL PROPERTY SHOWN ON THIS PLAT AND STATE THAT WE ARE THE LEGAL OWNERS THEREOF AND THAT THE INFORMATION SHOWN HEREON IS TRUE AND CORRECT TO THE BEST OF OUR KNOWLEDGE AND BELIEF.

*Paul Owhadi*  
 PAUL OWHADI

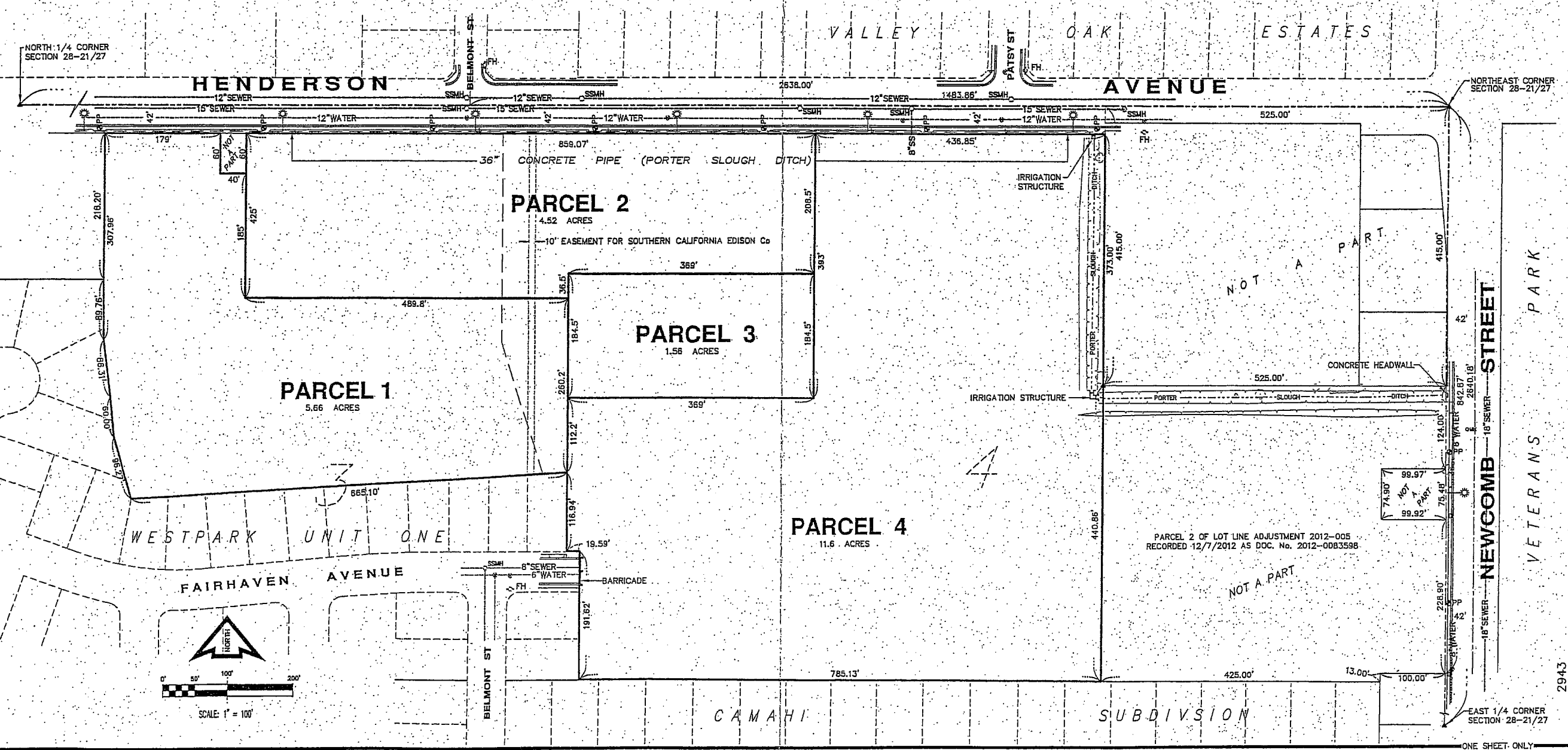


EXHIBIT F

RESOLUTION NO. \_\_\_\_\_

A RESOLUTION OF THE PARCEL MAP COMMITTEE OF THE CITY OF PORTERVILLE  
CONTAINING FINDINGS IN SUPPORT OF APPROVAL OF TENTATIVE PARCEL MAP  
2012-002-PM TO DIVIDE A 23± ACRE SITE INTO FOUR (4) PARCELS GENERALLY  
LOCATED AT THE SOUTH WEST CORNER OF HENDERSON AVENUE AND  
NEWCOMB STREET

WHEREAS: The City Council of the City of Porterville at its scheduled meeting of March 5, 2013, conducted a public hearing to consider Tentative Parcel Map PRC-2012-002-PM; and

WHEREAS: The City Council received testimony from all interested parties relative to said Tentative Parcel Map; and

WHEREAS: The Parcel Map Committee of the City of Porterville at its regularly scheduled meeting of February 13, 2013, conducted a public hearing to consider approval of Tentative Parcel Map 2012-002-PM to divide a 23.4± acre vacant site into four (4) parcels as follows:

Parcel 1 – 5.70± acres

Parcel 2 – 4.49± acres

Parcel 3 – 1.57± acres

Parcel 4 – 11.69± acres

; and

WHEREAS: The Parcel Map Committee discussed concerns and conditions relevant to the proposed project; and

WHEREAS: On February 5, 2013, the Environmental Coordinator made a preliminary determination that in compliance with the California Environmental Quality Act, a Mitigated Negative Declaration would be appropriate for the proposed project; and

WHEREAS: The Parcel Map Committee provided the opportunity to the public and all interested parties to comment on the proposed tentative subdivision map; and

WHEREAS: The Parcel Map Committee made the following findings:

1. That the proposed tentative parcel map is contingent upon approval of General Plan Amendment PRC 2012-002-G and Zone Change PRC 2012-002-Z.
2. That the proposed parcel map is consistent with applicable general and specific plans.

**ATTACHMENT  
ITEM NO. 6**

3. The proposed use of the site shall be required to be operated and maintained to comply with State Law, the City of Porterville Development Ordinance, adopted Building Codes and all other applicable laws and ordinances.
4. The design and improvements of the parcel map and the proposed project are consistent with the proposed Commercial Mixed Use General Plan designation, as represented in Exhibits A & B.
5. That the site is physically suitable for the type and density of the proposed development.

The subject site is flat. Conformance with the City Storm Drain Master Plan, and requirements relative to grading, the California Building Code, etc., will be required. Additionally, compliance with federal, State and local regulation requiring that storm water runoff monitored and maintained free of heavy concentration of pollutants will mitigate this potential impact to a level of insignificance (NPDES standards).

6. That the design of the parcel map or proposed improvements will not cause substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat.
7. That the design of the parcel map or type of improvements will not cause serious public health problems.

The location of the project and the conditions under which it would be operated or maintained will not be detrimental to the public health, safety or welfare, or materially injurious to properties or improvements in the area.

8. That the design of the parcel map or the type of improvements will not conflict with easements, acquired by the public at large, for access through or use of, property within the proposed parcel map.
9. That the Parcel Map Committee reviewed the proposed parcel map and recommended that the City review the project in its entirety.

NOW, THEREFORE, BE IT RESOLVED: That the City Council does hereby approve Tentative Parcel Map 2012-002 subject to the following conditions:

1. An easement shall be recorded prior to the issuance of the first building permit that provides ingress/egress, parking, trash and connectivity throughout the development. Such an easement between Parcels 1 and 2 shall serve as a guarantee for the functionality and circulation of those parcels as proposed. Also shown on Exhibit A, a drive aisle between Parcels 1 and 2 shall be fully developed and provide connectivity from the secondary exit of the mini-storage to the mini-storage main entrance when Parcel 1 is developed.

2. An easement shall be recorded prior to the issuance of the Parcel 4 building permit that provides for emergency vehicular access to Newcomb Street over and across the adjacent easterly parcel.
3. As development of any parcel occurs, all right of way improvements along that parcel's frontage shall be constructed in accordance with the mitigation measures and recommendations of the Ruettgers & Schuler traffic study (2012), as represented in Exhibit A and identified in the resolution approving the Mitigated Negative Declaration, prepared pursuant to CEQA. In the event Parcel 3 should develop prior to Parcel 2, the frontage improvements shall be completed for Parcel 2 at that time. At such time as Parcel 4 is developed, the off-site improvements extending the eastbound left turn lane on Henderson Avenue shall be constructed. At such time as Parcel 2 is developed, the off-site improvements extending the eastbound left turn lane on Henderson Avenue and the median improvements further west as represented on the site plan shall be constructed. In the event Parcel 1 is developed prior to Parcel 2, the raised median island improvements on Henderson Avenue shall be constructed.
4. Due to potential drainage issues, prior to securing a building permit, a grading sewer and water, and drainage master plan for Parcels 1, 2, 3, and 4, shall be submitted for review and approval by the City Engineer. As development of the site occurs, the phased master plan shall be implemented as approved by the City Engineer.
5. Prior to issuance of any building permit for the Project, access easements for ingress/egress, parking, and trash pick-up shall be recorded as deemed necessary by the City Engineer and Community Development Director. Copies of the easements shall be provided to the City.
6. Gates that have restricted access shall be identified on building permit plans.
7. The automated gate between Retail "B" and the mini-storage shall allow traffic to exit from the commercial complex to Henderson Avenue only. No access into shopping center is permitted.
8. The developer/applicant shall comply with the City Master Plans, Standard Specifications for Public Works Construction (2009 Edition), and Standard Plans and Specifications (2007 Edition), except where they are in conflict with the Americans with Disabilities Act and the 2010 California Building Code, the Tulare County Hazardous Waste Management Plan, the California Manual on Uniform Traffic Control Devices, the Porterville Circulation Element, and the Tulare County Congestion Management Program.
9. The developer/applicant shall pay all applicable fees according to the Municipal Code and State law. The developer/applicant intends to utilize the 10-year development fee payment plan for all fees except school fees pursuant to Resolution 37-2012, adopted April 17, 2012.

10. The developer/applicant shall reimburse the City for constructed frontage improvements along Henderson Avenue in accordance with approved resolutions.
11. The developer/applicant shall construct and/or repair street, curb, gutter, sidewalk, etc. along the Henderson Avenue frontage, except where they exist and are in good condition in the opinion of the City Engineer at the time of inspection prior to building permit issuance (Ordinance. No. 1306).
12. The developer/applicant shall construct sidewalk necessary to obtain a width of 9.5 feet along the full frontage of the proposed project.
13. The developer/applicant shall construct drainage facilities as required to serve the property (Ordinance. No. 1306). The parking lot shall be designed to convey water to the City drainage system without crossing driveways.
14. The Porter Slough Ditch runs through this property, requiring piping and reservation of an easement in favor of the irrigation company. Construction of structures within the easement area shall be prohibited.
15. The developer/applicant shall coordinate with the Porterville Irrigation District and City of Porterville regarding potential conflicts with the existing pipeline/open ditch that crosses the property. The Porterville Irrigation District shall specify appropriate pipeline replacement size(s) and materials, should replacement or installation be mandated.
16. The developer/applicant shall, under City inspection, remove all existing abandoned and unnecessary items, to the satisfaction of the City Engineer, before the issuance of a certificate of occupancy (e.g., foundations, septic tanks, irrigation pipes, etc.).
17. The developer/applicant shall abandon existing wells, if any, after first getting an abandonment permit from the Tulare County Environmental Health Department, and providing the City Engineer with proof of compliance with County regulations before performing any grading or issuance of the building permit, whichever comes first.
18. The developer/applicant shall construct the pipe connecting to on-site fire hydrants to City water main standards and shall provide easements for maintenance to the on-site water mains.
19. At the time of approval of the Final Map the developer/ applicant shall dedicate right-of-way for a street width that matches the ultimate width in the adopted Land Use and Circulation Elements and/or that was established by the City Council, along with dedication of right-of-way adequate for all improvements associated with the project Traffic Study prepared by Ruetters and Schuler, as well as dedication of property required for ADA ramp(s) (Section 21-23). Dedicate required utility easements.
20. The developer/applicant shall comply with Chapter 7, Article XIII of the City Code and Chapter 18 and Appendix J of the California Building Code and provide a Preliminary

Soils Report (C.C. Sec. 7-126 & Res. 4997) including results of "R-Value" tests and recommendations regarding construction of public improvements that address City Standard C-13, satisfactory to the City Engineer, prior to the approval of the improvement plans or start of grading, whichever comes first. Additional reporting requirements are as indicated below:

- a. Final Grading, Drainage and Soils Report, prior to issuance of building permits (C.C. Sec. 7-133);
  - b. Soils Report(s) in accordance with Chapter 18 and Appendix J of the California Building Code.
21. Prior to approval of a permit or other grant of approval for development of the property, the developer/applicant shall comply with or provide plans that comply with City Retaining Wall Standards at Lot Lines (adopted by City Council January 3, 1989).
22. The dedication of easements or any other potential dedications shall be clearly identified on the Parcel Map. A Dedications Statement shall be placed on the map that reads:
- “Pursuant to the authority conferred by the City of Porterville, Ordinance No. 1590, adopted February 20, 2001, the undersigned, on behalf of the Public and City Council of the City of Porterville consents to the acceptance and recordation of the \_\_\_\_\_ dedication(s) as shown on this map.  
Dated this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_  
By Michael K. Reed, City Engineer PLS 7514”
23. Prior to recording the Parcel Map, the developer/applicant shall provide improvements by the method indicated below:
- a. Simultaneous recording of a separate legal instrument or placement of a statement on the Parcel Map, if appropriate, that reads as follows:

“In accordance with Section 66411.1 of the Government Code, the construction of on-site and off-site improvements, such as but not limited to, curbs, gutters, sidewalks, paveouts, pavement transitions, traffic signage and markings, median islands and the mitigation measures described in the project Traffic Study prepared by Ruettgers and Schuler Civil Engineers, utilities, grading, parking lot improvements, well abandonment (if any), removal of irrigation lines (if any), etc. has been deferred until such time as a permit or other grant of approval for development of the parcel is issued. Such construction may be phased when only a portion of the parcels shown herein receive such approval. The phased construction shall consist of all improvements related to the specific parcels plus that required to make the installed improvements function.”
24. The developer/applicant shall move existing utility structures, if any, that are in conflict with the project driveways and associated transitional traffic lanes (for example, poles, splice boxes, vaults, etc.) to a position that provides a minimum of four feet (4') of clear

space in the sidewalk area and a minimum of two feet (2') of clear space from the curb face to the structure, unless they are below grade (Title 24 OSA).

25. Concurrent with issuance of building permits and construction of any improvements on a parcel, the developer/applicant shall have constructed, or pay fees for, street lights on Marbelite poles complying with Southern California Edison Company specifications, as approved by the City Engineer. Use of wood poles is prohibited without prior written approval of the City Engineer.
26. Prior to recording the final map, the developer/applicant shall provide easements, permits (from other agencies as required), calculations, etc., if, in the opinion of the City Engineer, they are needed for the proper functioning or phasing of the development (e.g., water, sewer, drainage, "turn arounds", etc.). If the City Engineer determines that construction of improvements is needed to protect public health and safety or for orderly development of the surrounding area, the developer/applicant shall construct or provide a surety for said improvements.
27. Prior to recording the final map, the developer/applicant shall replace or provide surety for replacement of irrigation pipes in the right-of-way (if present) if, in the opinion of the City Engineer, replacement is warranted. The developer/applicant shall cure any leaks in irrigation pipes crossing the subject parcel, if in the opinion of the City Engineer, such leaks may interfere with the development of the subject site. Easements shall be provided for irrigation pipes across parcels created, if such pipes will continue in use.
28. The developer/applicant shall dedicate a one foot (1') limitation of access strip between driveways shown on Exhibit A and those locations where, in the opinion of the City Engineer, it is undesirable to allow access. Dedication shall be illustrated on the Parcel Map or by recordation of a separate legal document.
29. The developer/applicant shall construct a City standard barricade or wall at the end of all dead end streets or drive aisles, with the exception of Fairhaven Avenue, where an emergency access gate shall be constructed and keyed with a Knox box padlock.
30. Depending on the location of the existing fire hydrant(s), additional fire hydrants may be required. All hydrants must be in place and accepted by the Fire Department prior to any combustibles being brought onto the site.
31. The City will test and maintain all fire hydrants in the City whether on private property or not. An "easement" is required from the owner.
32. Fire hydrant spacing shall be as follows:
  - a. In Commercial development, one hydrant shall be installed at 300-foot intervals.
  - b. In Residential development, one hydrant shall be installed at 500-foot intervals.

33. Access roads shall be provided for every facility, building or portion of a building, when any portion of the facility or portion of an exterior wall is located more than 150 feet from a fire apparatus access as measured by an approved route around the exterior of the building.
34. Project must meet minimum fire flow requirements per the table in Appendix B & C of the California Fire Code. Fire flow for the project as proposed would be 1,500 GPM.
35. Areas identified as "Fire Lanes" must be identified as such by red painted curbs and identified per requirements set forth in the California Vehicle Code Section 22500.1.
36. The project shall comply with all local, State, and federal laws.
37. The project shall comply with all mitigation measures identified in the approved CEQA document.

PASSED, APPROVED AND ADOPTED this \_\_\_\_\_ day of March, 2013.

By: \_\_\_\_\_  
Virginia R. Gurrola, Mayor

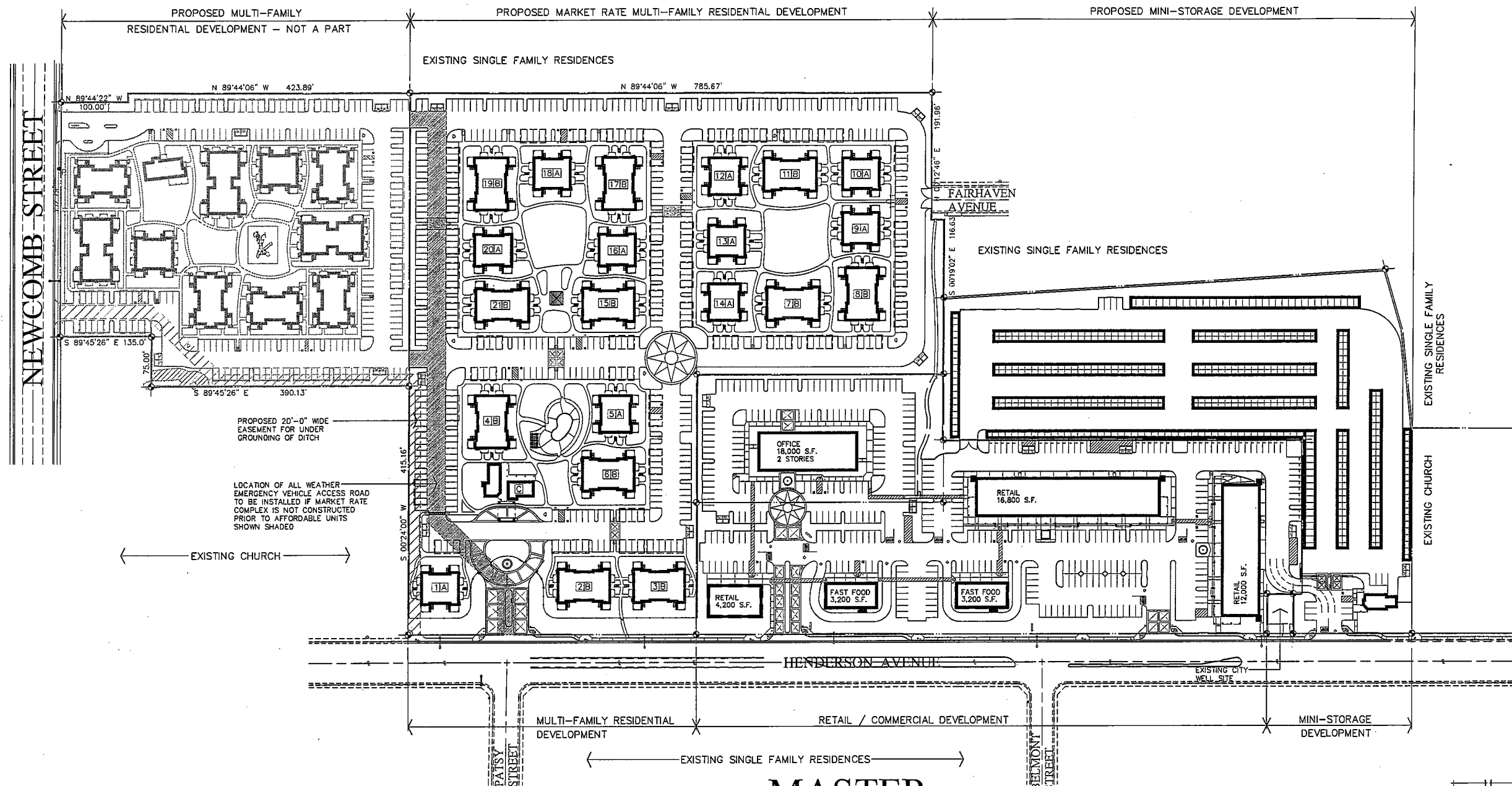
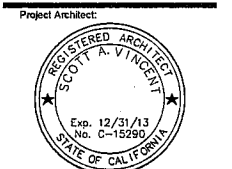
ATTEST:  
John D. Lollis, City Clerk

By: \_\_\_\_\_  
Patrice Hildreth, Chief Deputy City Clerk



Revisions	Date

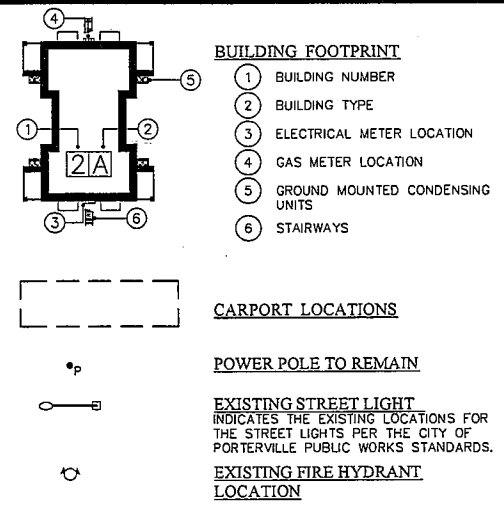
PROPOSED DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
 WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA



# MASTER DEVELOPMENT PLAN

1" = 80'-0"

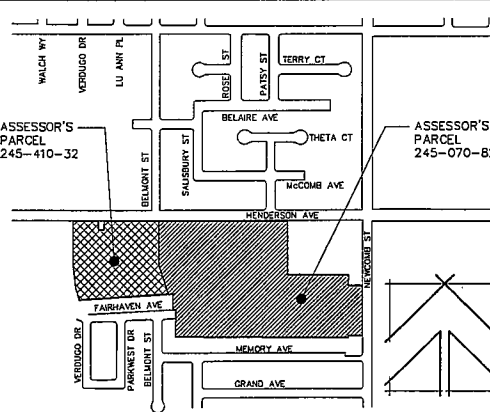
## SYMBOLS



## STATISTICS

MULTI-FAMILY RESIDENTIAL		MASTER DEVELOPMENT	
<b>PROJECT PROPOSAL</b>	178 UNIT MARKET RATE MULTI-FAMILY RESIDENTIAL DEVELOPMENT	<b>PROJECT PROPOSAL</b>	A MIXED USE DEVELOPMENT INCLUDING 258 MULTI-FAMILY RESIDENTIAL UNITS; A 39,000 S.F. RETAIL CENTER; AND A 579 UNITS MINI-STORAGE FACILITY
<b>SITE AREA</b>	506,403 S.F. (11.6 ACRES)	<b>ASSESSOR'S PARCELS</b>	245-070-82 AND 245-410-32
<b>PROJECT COVERAGE</b>	108,128 S.F. (21.4%)	<b>SITE AREA</b>	a. PARCEL 245-070-82 919,116 S.F. (21.10 ACRES) b. PARCEL 245-410-32 321,908 S.F. ( 7.39 ACRES) c. TOTAL SITE AREA 1,241,024 S.F. (28.49 ACRES)
a. BUILDING AREA (INCLUDES CARPORTS)	153,456 S.F. (30.3%)	<b>PROPOSED ZONINGS</b>	a. MULTI-FAMILY RESIDENTIAL DEVELOPMENT RM-3/PD b. RETAIL CENTER DEVELOPMENT CN/PD c. MINI-STORAGE DEVELOPMENT CN/PD
b. PARKING / PAVED AREA	244,819 S.F. (48.3%)	<b>PROPOSED PROJECT AREAS</b>	a. MULTI-FAMILY RESIDENTIAL DEVELOPMENT 729,194 S.F. (16.74 ACRES) b. RETAIL CENTER DEVELOPMENT 195,584 S.F. ( 4.49 ACRES) c. OFFICES DEVELOPMENT 68,390 S.F. ( 1.57 ACRES) d. MINI-STORAGE DEVELOPMENT 247,856 S.F. ( 5.70 ACRES)
c. LANDSCAPE AREA	14,448 S.F. ( 2.9%)	<b>UNIT STATISTICS</b>	a. (96) UNITS TYPE 'J' 1 BEDROOM, 1 BATH GARDEN WALK-UP 766 S.F. b. (40) UNITS TYPE 'K' 2 BEDROOM, 2 BATH GARDEN WALK-UP 1,067 S.F.
<b>PROPOSED PROJECT DENSITY</b>	14.48 UNITS / ACRE	<b>BUILDING AREA CALCULATIONS</b>	a. (10) BUILDING TYPE 'A' (7,492 S.F. PER BUILDING) 74,920 S.F. d. (11) BUILDING TYPE 'B' (9,396 S.F. PER BUILDING) 103,356 S.F. e. TOTAL RESIDENTIAL AREA NON-RESIDENTIAL BUILDINGS 178,276 S.F.
<b>REQUIRED PARKING</b>		<b>TOTAL BUILDING AREA</b>	214,362 SO.FT.
a. RESIDENT PARKING		<b>GROUND FLOOR AREA</b>	125,224 SO.FT.
1. COVERED PARKING	168 STALLS		
2. OPEN STALLS	128 STALLS		
b. GUEST PARKING	56 STALLS		
c. TOTAL PARKING REQUIRED	352 STALLS		
d. ACCESSIBLE STALLS			
1. COVERED STALLS @ 2%	4 STALLS		
2. OPEN STALLS @ 5%	9 STALLS		
<b>PARKING PROVIDED</b>			
a. CARPORTS	168 STALLS		
b. OPEN STALLS	268 STALLS		
c. TOTAL PARKING PROVIDED	436 STALLS		
d. ACCESSIBLE STALLS			
1. COVERED STALLS	4 STALLS		
2. OPEN STALLS	9 STALLS		

## VICINITY MAP



<b>ISSUANCES:</b>	<b>DATE:</b>
● DESIGN REVIEW	03.08.12
○ PLAN CHECK	
○ BACK CHECK	
○ PERMITS	
○ CONSTRUCTION	
○ AS BUILT	

All drawings and written material appearing herein constitute original work of the architect and may not be duplicated, used, or disclosed without written consent of the architect.

Reference North:

Scale: 1" = 80'-0"

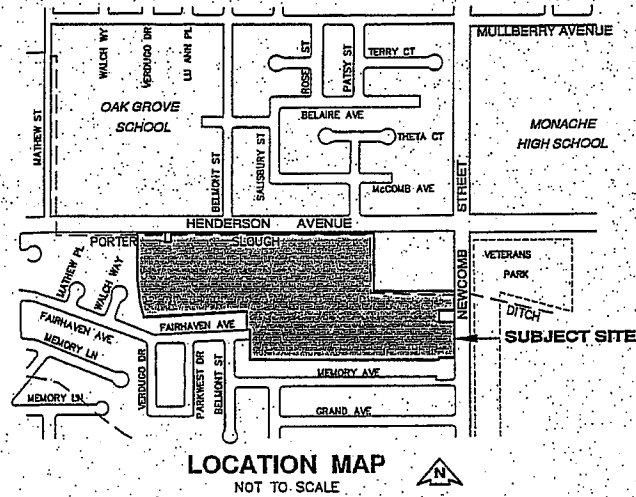
Project Name: PACIFIC RIM PORTERVILLE, CA.

Project Number: 110926

Plot Date: 01.30.13

Sheet Number:

EXHIBIT A



- NOTES**
1. DATE: AUGUST 15, 2012 REVISED JANUARY 26, 2013
  2. APN: 245-070-082, 245-410-032
  3. EXISTING USE: VACANT
  4. PROPOSED USE: MEDIUM DENSITY RESIDENTIAL AND COMMERCIAL
  5. DRAINAGE: MAINTAIN EXISTING PATTERNS, UNLESS OTHERWISE NOTED.
  6. DOMESTIC WATER: CITY OF PORTERVILLE.
  7. SANITARY SEWER: CITY OF PORTERVILLE.
  8. NATURAL GAS: THE GAS COMPANY
  9. POWER: SOUTHERN CALIFORNIA EDISON
  10. TELEPHONE: A.T. & T.
  11. CABLE TV: CHARTER COMMUNICATIONS
  12. FLOODING: ZONE X, PER FIRM.
  13. PROPOSED IMPROVEMENTS, PUBLIC UTILITIES AND TIME OF COMPLETION: AS SHOWN OR AS REQUIRED BY CITY OF PORTERVILLE.

**LEGEND**

	PROPERTY BOUNDARY
	SIGN
	MANHOLE
	POWER POLE
	WATER METER
	WATER VALVE
	FIRE HYDRANT
	STREET LIGHT
	BUSH
	TREE

PARCEL No. PER PARCEL MAP No. 2806

# TENTATIVE PARCEL MAP

BEING A DIVISION OF PARCEL 4 AND A PORTION OF PARCEL 3 OF PARCEL MAP No. 2806 SITUATED IN THE NORTH EAST 1/4 SECTION 28, T.21S., R.27E., M.D.B. & M. IN THE CITY OF PORTERVILLE, COUNTY OF TULARE, STATE OF CALIFORNIA.

For: **PAUL & SUSAN OWHADI**  
1470 W. HERNDON AVENUE  
FRESNO, CALIFORNIA 93711  
(424) 234-5555

By: **JAMES WINTON & ASSOCIATES**  
150 WEST MORTON AVENUE  
PORTERVILLE, CALIFORNIA 93257  
(559) 781-2700

**OWNER'S STATEMENT**

WE HEREBY APPLY FOR APPROVAL OF THE DIVISION OF REAL PROPERTY SHOWN ON THIS PLAT AND STATE THAT WE ARE THE LEGAL OWNERS THEREOF AND THAT THE INFORMATION SHOWN HEREON IS TRUE AND CORRECT TO THE BEST OF OUR KNOWLEDGE AND BELIEF.

By: *Paul Owhadi*  
PAUL OWHADI

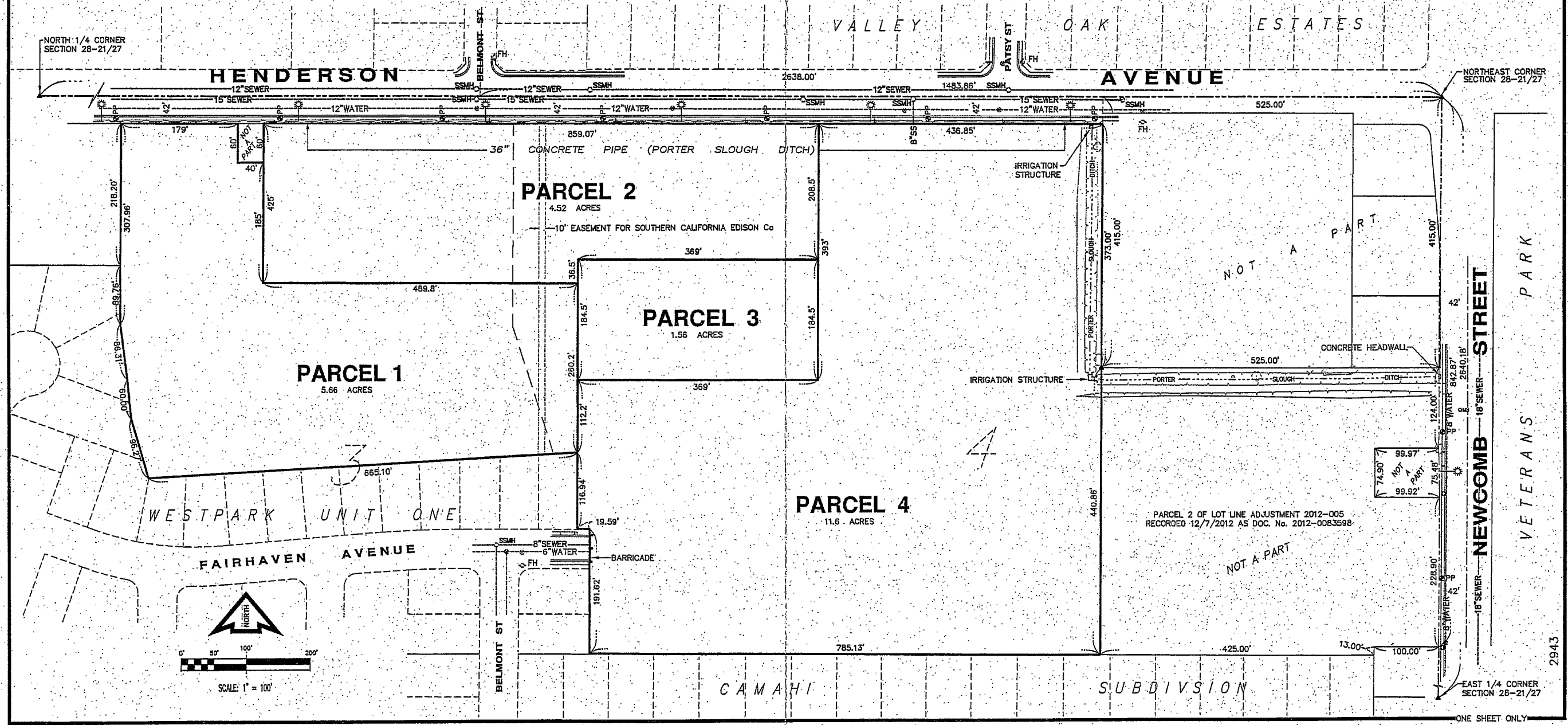


EXHIBIT B

RESOLUTION NO. \_\_\_\_\_

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF PORTERVILLE  
CONTAINING FINDINGS IN SUPPORT OF APPROVAL OF A  
CONDITIONAL USE PERMIT (2012-002-C)  
FOR A PERSONAL STORAGE FACILITY ON A 5.66± ACRE PORTION OF  
THE PROPOSED PACIFIC RIM COMMERCIAL MIXED USE PROJECT

WHEREAS: The City Council of the City of Porterville at its regularly scheduled meeting of March 5, 2013, conducted a public meeting to consider approval of a Conditional Use Permit to accommodate a 518 unit personal storage facility in the Commercial Mixed Use zone on a 5.66± acre parcel; and

WHEREAS: The project site is currently undeveloped; and

WHEREAS: On February 5, 2013, the Environmental Coordinator made a preliminary determination that a Mitigated Negative Declaration would be appropriate for the proposed project; and

WHEREAS: The proposed Conditional Use Permit is supported by the General Plan and Development Ordinance in that the proposed development is consistent with Section 301.13 of the Porterville Development Ordinance (Chapter 21 of the Porterville Municipal Code).

NOW, THEREFORE, BE IT RESOLVED that the City Council of the City of Porterville does hereby make the following findings:

1. The General Plan designation for the subject area was approved by the City Council on March 5, 2013, modifying the General Plan designation from RM-3 (High Density Residential), RM-2 (Medium Density Residential), and CN (Neighborhood Commercial) to CMX (Commercial Mixed Use) for the subject parcel.
2. The Zoning classification for the subject area was approved by the City Council on March 5, 2013, modifying the zoning from RM-3 (High Density Residential), RM-2 (Medium Density Residential), and CN (Neighborhood Commercial) to CMX (Commercial Mixed Use) for the subject parcel.
3. That a Negative Declaration was prepared for the project in accordance with the California Environmental Quality Act and was transmitted to interested agencies and made available for public review and comment. The review period ran for twenty (20) days, from February 8, 2013, to February 28, 2013.
4. The City Council is the decision-making body for the project.

**ATTACHMENT  
ITEM NO. 7**

BE IT FURTHER RESOLVED: That the City Council does hereby approve the Conditional Use Permit for development of a 518 unit personal storage facility as represented and incorporated herein as Exhibit "A" subject to the following conditions:

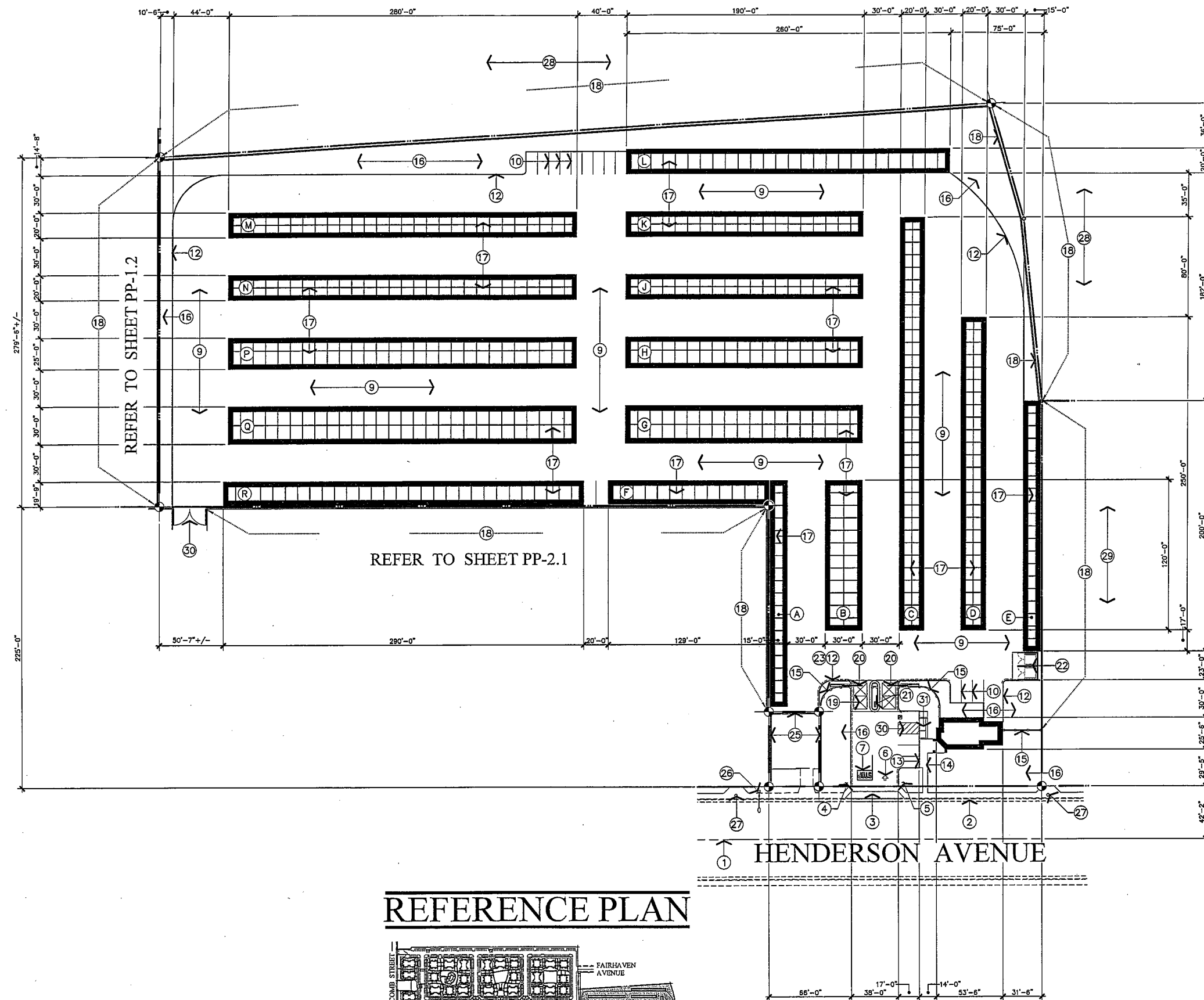
1. The proposed project shall comply with all local, State, and Federal laws, including but not limited to Section 301.13 of the Porterville Development Ordinance.
2. The project shall be developed in a manner consistent with the site plan and elevations, included herein as Exhibits "A" and "B".
3. The personal storage units shall prohibit storage of hazardous materials on-site.
4. The Zoning Administrator shall consider any modifications to the development for consistency.

PASSED, APPROVED AND ADOPTED this 5<sup>th</sup> day of March 2013.

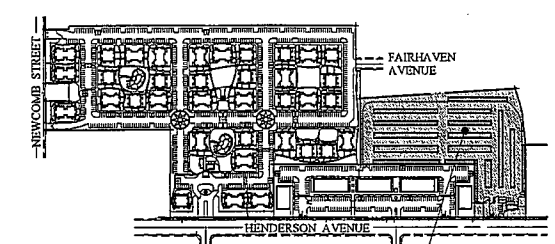
By: \_\_\_\_\_  
Virginia R. Gurrola, Mayor

ATTEST:  
John D. Lollis, City Clerk

By: \_\_\_\_\_  
Patrice Hildreth, Chief Deputy City Clerk



### REFERENCE PLAN



NOTE: SHADING INDICATES THE PORTION INCLUDED ON THIS PLAN

# MINI-STORAGE SITE PLAN

1" = 40'-0"

## STATISTICS

PROPOSED USE	NEIGHBORHOOD MINI STORAGE		
PROJECT ZONING			
SITE AREA	257,276 S.F. (5.90 ACRES)		
<b>SITE COVERAGES</b>			
a. BUILDING AREA	1,290 S.F. (0.5%)		
b. PAVED AREA	143,314 S.F. (55.7%)		
c. LANDSCAPE AREA	112,672 S.F. (43.8%)		
<b>BUILDING AREA(S)</b>			
1. STORAGE BUILDING			
<b>BUILDING</b>	<b>USE/UNIT SIZE</b>	<b>NO. OF UNITS</b>	<b>BUILDING AREA</b>
A	10 x 15	18	2,700 S.F.
B	10 x 15	24	3,600 S.F.
C	10 x 10	56	5,600 S.F.
D	10 x 10	50	5,000 S.F.
E	10 x 15	20	3,000 S.F.
F	10 x 20	13	2,600 S.F.
G	10 x 15	38	5,700 S.F.
H	10 x 10	19	1,900 S.F.
J	10 x 15	19	2,850 S.F.
K	10 x 10	38	3,800 S.F.
L	10 x 10	38	3,800 S.F.
M	10 x 20	26	5,200 S.F.
N	10 x 10	56	5,600 S.F.
P	10 x 10	56	5,600 S.F.
Q	10 x 10	28	2,800 S.F.
R	10 x 15	28	4,200 S.F.
	10 x 15	56	8,400 S.F.
	10 x 20	29	5,800 S.F.
<b>TOTAL UNITS/AREA</b>		<b>622 UNITS</b>	<b>79,150 S.F.</b>
2. OFFICE BUILDING AREA			1,290 SQ. FT.
<b>PARKING REQUIRED</b>			
a. 1 STALL / 75 UNITS			14 STALLS
b. 1 STALL / 300 S.F. OFFICE			4 STALLS
c. TOTAL STALLS REQUIRED			18 STALLS
d. ACCESSIBLE STALLS			1 STALL
<b>PARKING PROVIDED</b>			
a. OPEN STALLS			17 STALLS
b. ACCESSIBLE STALLS			1 STALL

## KEYNOTES

- 1 INDICATES SECTION LINE/ CENTERLINE OF THE STREET.
- 2 EXISTING RIGHT-OF-WAY IMPROVEMENTS ALONG THE STREET FRONTAGE INCLUDING CONCRETE CURB, GUTTER, AND SIDEWALK, PER CITY OF PORTERVILLE PUBLIC WORKS DEPARTMENT STANDARDS TO REMAIN.
- 3 INSTALL COMMERCIAL STREET-TYPE APPROACHES PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 4 INSTALL 30" STATE STANDARD "STOP" SIGN(S) AT LOCATIONS SHOWN. SIGN SHALL BE MOUNTED ON A 2 INCH DIAMETER GALVANIZED POST WITH THE BOTTOM OF THE SIGN 7'-0" ABOVE GROUND AND LOCATED BEHIND CURB AND IMMEDIATELY BEHIND MAJOR STREET SIDEWALK. "RIGHT TURN ONLY" SIGN AT SAME LOCATION. INSTALL 30"x36" STATE STANDARD SIGN IMMEDIATELY BELOW THE STOP SIGN ON THE SAME POST/ TYPICAL.
- 5 PROVIDE SIGN AT EACH SITE ENTRANCE THAT CLEARLY STATES, 1" HIGH LETTERING STATING, "UNAUTHORIZED VEHICLES PARKED IN DESIGNATED ACCESSIBLE SPACES NOT DISPLAYING DISTINGUISHING PLACARDS OR SPECIAL LICENSE PLATES ISSUED FOR PERSONS WITH DISABILITIES WILL BE TOWED AWAY AT OWNER'S EXPENSE. TOWED VEHICLES MAY BE RECLAIMED BY CONTACTING THE CITY OF SAN JOSE POLICE DEPARTMENT.
- 6 INDICATES LOCATION OF NEW PAINTED TRAFFIC DIRECTIONAL ARROWS PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 7 INDICATES LOCATION OF NEW PAINTED TRAFFIC SIGNAGE PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 8 INDICATES LOCATION OF PROPOSED POLE MOUNTED PROJECT SIGN (TO BE REVIEWED AND APPROVED UNDER SEPARATE PERMIT). COORDINATE INSTALLATION OF POWER SUPPLY TO THIS LOCATION WITH THE ELECTRICAL SITE PLAN/ TYPICAL.
- 9 INSTALL NEW CAST-IN-PLACE CONCRETE PAVING OVER COMPACTED NATIVE SOIL. COORDINATE THE INSTALLATION REQUIREMENTS WITH THE PAVING PLAN/ TYPICAL.
- 10 INSTALL 4 INCH WIDE WHITE PAINTED STALL STRIPING PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 11 INDICATES LOCATION OF ACCESSIBLE PARKING STALL WITH 8'-0" WIDE UNLOADING AREA AND WARPED WALK WHEELCHAIR RAMP. INSTALL SIGNAGE AND MARKING AS REQUIRED BY TITLE 24 CCR ACCESSIBILITY REQUIREMENTS/ TYPICAL.
- 12 INDICATES LOCATION OF 6 INCH CONTINUOUS CURB/ TYPICAL.
- 13 6 INCH CONTINUOUS CONCRETE CURB WITH INTEGRAL 4 INCH THICK CONCRETE WALK/ TYPICAL.
- 14 INDICATES NEW 4 INCH THICK CONCRETE WALK WITH NON-SLIP FINISH PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 15 INDICATES LOCATION OF 6'-0" HIGH REINFORCED CONCRETE MASONRY FENCE (GROUT SOLID) WITH CEMENT PLASTER FINISH AND PILASTERS AT 20'-0" ON CENTER (INCREASE HEIGHT TO 8'-0" AT STORAGE BUILDINGS)/ TYPICAL.
- 16 LANDSCAPE AREA(S). COORDINATE THE WORK WITH THE CONCEPT LANDSCAPE PLAN/ TYPICAL.
- 17 STORAGE UNITS SHALL BE PRE-FABRICATED STEEL STORAGE UNITS TO BE INSTALLED UNDER SEPARATE BUILDING PERMITS/ TYPICAL.
- 18 INSTALL 6'-0" HIGH SOLID GROUTED REINFORCED CONCRETE MASONRY FENCE AT PROPERTY LINE/ TYPICAL.
- 19 INDICATES LOCATION OF COLORED CONCRETE ACCENT PAVING WITH TOOLED JOINT PATTERN AS INDICATED. COORDINATE INSTALLATION WITH THE GRADING AND DRAINAGE PLAN/ TYPICAL.
- 20 INDICATES THE LOCATION OF 6'-0" HIGH STEEL GATES WITH AUTOMATIC OPERATOR(S)/ TYPICAL.
- 21 INDICATES THE LOCATION OF PEDESTAL MOUNTED GATE CONTROLLER/ TYPICAL.
- 22 INDICATES LOCATION OF TRASH AND RECYCLING ENCLOSURE WITH REINFORCED CONCRETE MASONRY WALLS TO 4'-8" PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS/ TYPICAL.
- 23 DASHED LINE INDICATES LOCATION(S) OF CURBS TO BE PAINTED RED AND MARKED "FIRE LANE - NO PARKING". LETTERS SHALL BE WHITE, 3 INCHES HIGH AND SPACED AT 50'-0" ON-CENTER PER CITY OF PORTERVILLE FIRE DEPARTMENT STANDARDS/ TYPICAL.
- 24 INDICATES LOCATION OF 20'-0" WIDE x 6'-0" HIGH ORNAMENTAL WROUGHT IRON GATE(S) FOR EMERGENCY VEHICLE INGRESS AND EGRESS ONLY. INSTALLATION SHALL COMPLY WITH CITY OF PORTERVILLE PUBLIC WORKS AND FIRE DEPARTMENT REQUIREMENTS/ TYPICAL.
- 25 EXISTING CONCRETE MASONRY WALL AT CITY OF PORTERVILLE WATER WELL SITE TO REMAIN.
- 26 EXISTING STREET LIGHTS INSTALLED PER CITY OF PORTERVILLE PUBLIC WORKS STANDARDS TO REMAIN.
- 27 INDICATES THE LOCATION OF EXISTING POWER POLE TO REMAIN.
- 28 EXISTING ADJACENT SINGLE FAMILY RESIDENTIAL DEVELOPMENT TO REMAIN.
- 29 EXISTING ADJACENT CHURCH TO REMAIN.
- 30 8'-0" WIDE DISABLED ACCESSIBLE UNLOADING SPACE WITH RAMP ACCESS TO WALKWAY/ TYPICAL.
- 31 INSTALL ACCESSIBLE CURB RAMP WITH TRUNCATED DOMES/ TYPICAL.

## THE VINCENT COMPANY

ARCHITECT'S, INC.  
1500 West Shaw, Ste. 30  
Fresno, California 93711  
Phone: 559.225.2602

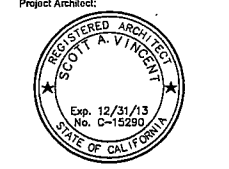
Revisions	Date
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PROPOSED DEVELOPMENT FOR:

# PACIFIC RIM COMPANIES

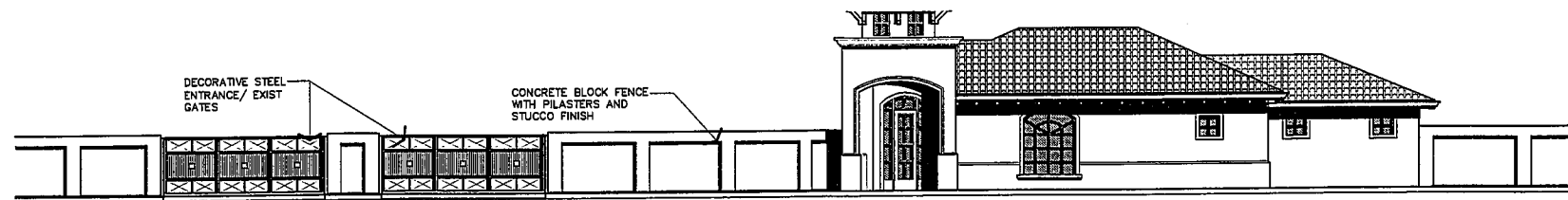
WEST HENDERSON AND NORTH NEWCOMB  
PORTERVILLE, CALIFORNIA

Project Architect:



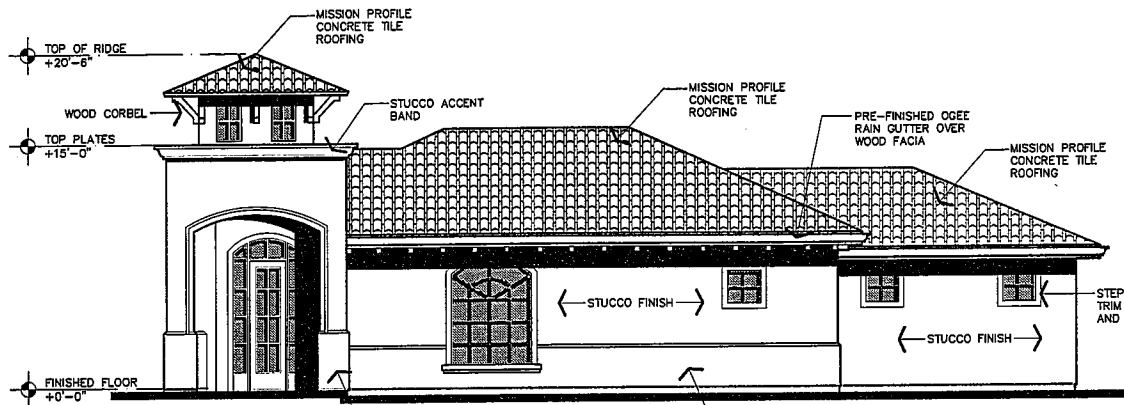
Issuances:	Date:
● DESIGN REVIEW	03.08.12
○ PLAN CHECK	
○ BACK CHECK	
○ PERMITS	
○ CONSTRUCTION	
○ AS BUILT	
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Reference North:	
Scale: 1" = 40'-0"	
Project Name:	PACIFIC RIM PORTERVILLE, CA.
Project Number:	110925
Plot Date:	03.06.12
Sheet Number:	
<b>PP3.1</b>	
Of	Sheets

EXHIBIT A



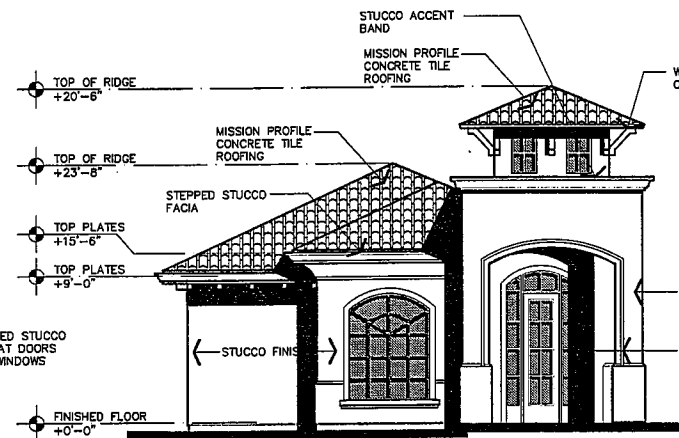
# ENTRANCE ELEVATION

1/8" = 1'-0"



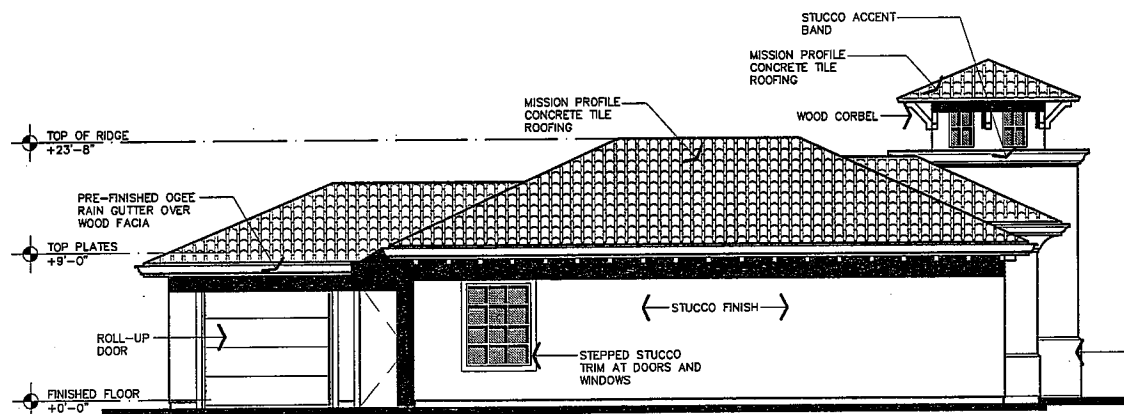
# OFFICE BUILDING NORTH ELEVATION

3/16" = 1'-0"



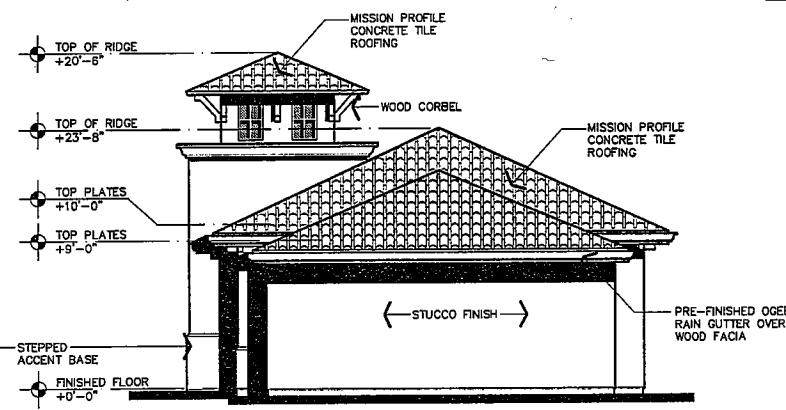
# OFFICE BUILDING EAST ELEVATION

3/16" = 1'-0"



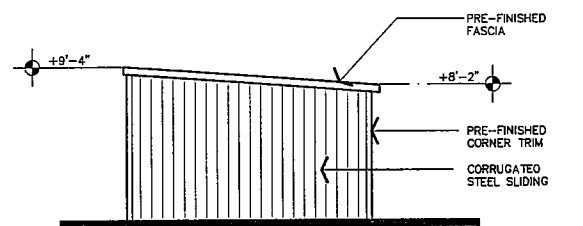
# OFFICE BUILDING SOUTH ELEVATION

3/16" = 1'-0"



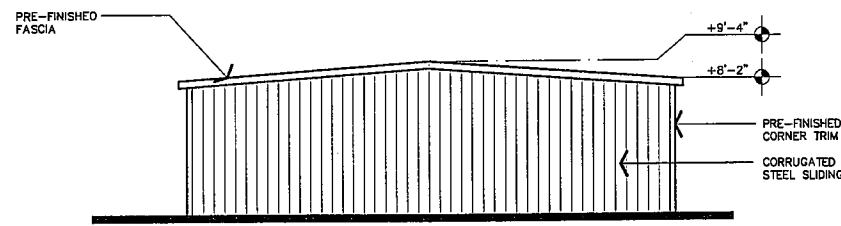
# OFFICE BUILDING WEST ELEVATION

3/16" = 1'-0"



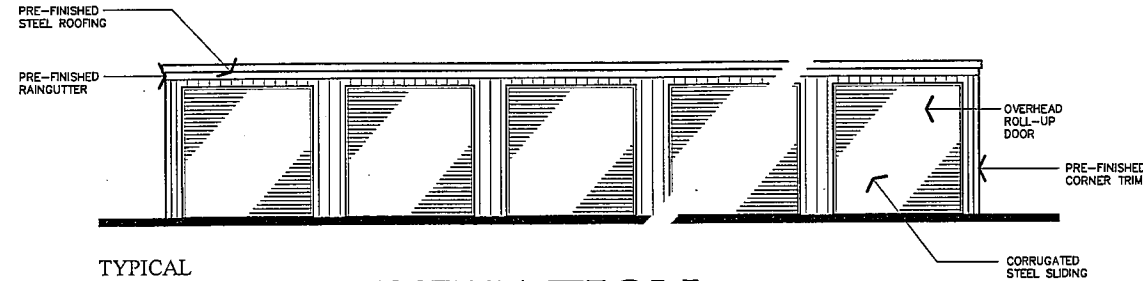
# SINGLE DEPTH END ELEVATION

3/16" = 1'-0"



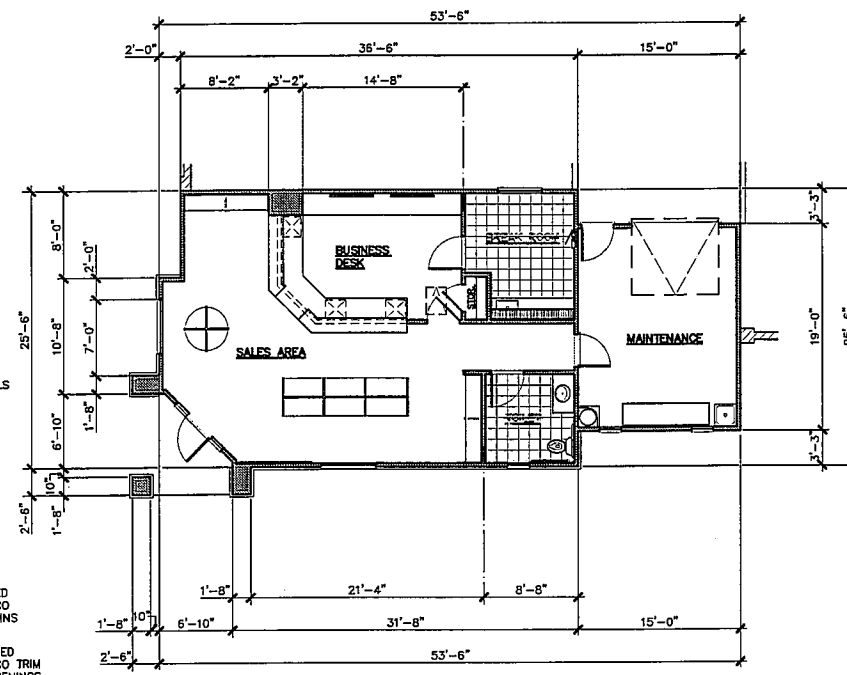
# DOUBLE DEPTH END ELEVATION

3/16" = 1'-0"



# TYPICAL FRONT ELEVATION

3/16" = 1'-0"



# OFFICE FLOOR PLAN

1/8" = 1'-0"

## BUILDING STATISTICS

OCCUPANCY GROUP	B
CONSTRUCTION TYPE	V-B
AREA CALCULATIONS	
a. SALES AREA	478 SQ. FT.
b. BUSINESS DESK	209 SQ. FT.
c. BREAK ROOM	114 SQ. FT.
d. TOILET/ HALL/ STORAGE	135 SQ. FT.
e. MAINTENANCE	288 SQ. FT.
f. COVERED PORCH	66 SQ. FT.
g. TOTAL BUILDING AREA	1,290 SQ. FT.

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Revisions	Date

PROPOSED MULTI-FAMILY DEVELOPMENT FOR:  
**PACIFIC RIM DEVELOPMENT COMPANY**  
 WEST HENDERSON AND NORTH NEWCOMB  
 PORTERVILLE, CALIFORNIA



Issuance:	Date:
DESIGN REVIEW	03.08.12
PLAN CHECK	
BACK CHECK	
PERMITS	
CONSTRUCTION	
AS BUILT	

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Reference North:  
  
 Scale: AS NOTED  
 Project Name: PACIFIC RIM  
 PORTERVILLE, CA.  
 Project Number: 110925  
 Plot Date: 10.17.12  
 Sheet Number:

**PP3.2**  
 Of . . . Sheets . . .

EXHIBIT B

Z:\2011\1025 - Porterville Apartments\13\_DESIGN\PROPOSAL\Site Plan\Rev\PP3.2 - Office Building.dwg, 10/20/12 12:20:00 AM, Any